

MANUFACTURERS' RECORD

A WEEKLY SOUTHERN INDUSTRIAL
RAILROAD AND FINANCIAL NEWSPAPER.

In an interview with
Hon. W. L. Trenholm,
published in this issue,
some striking facts are
presented bearing upon
the history of the silver
question and its relation
to the South.

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Baltimore, October 2, 1896.



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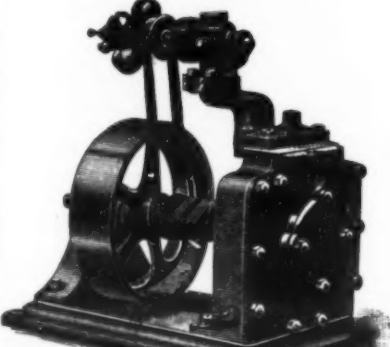
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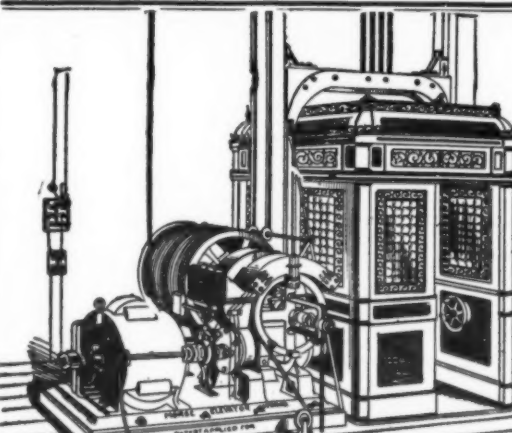
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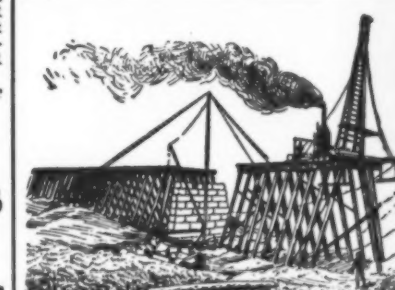
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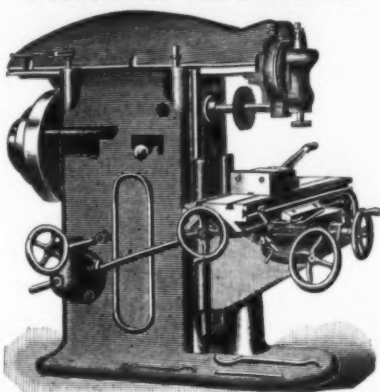
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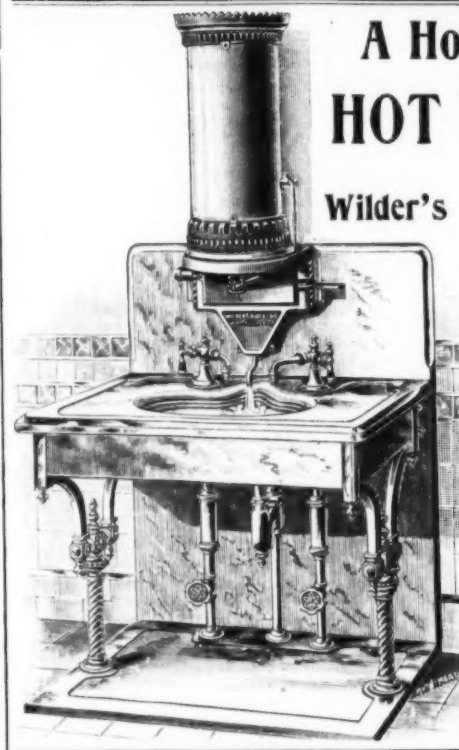
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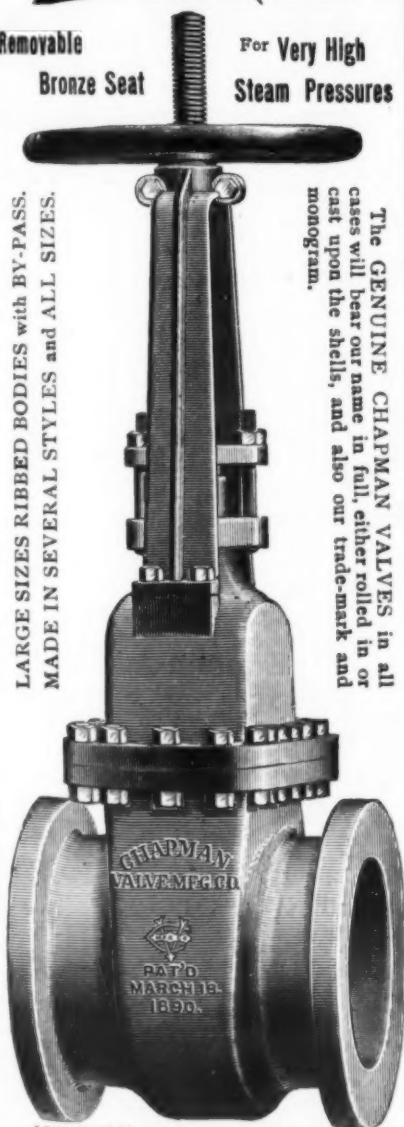
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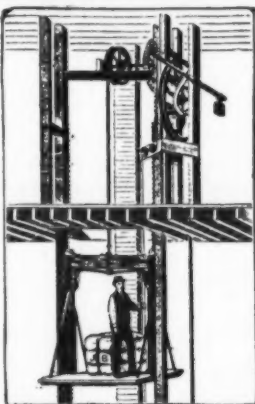
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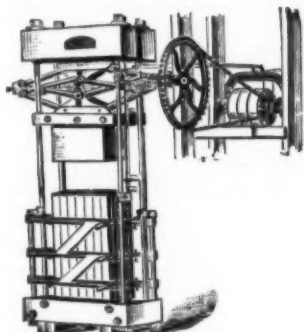
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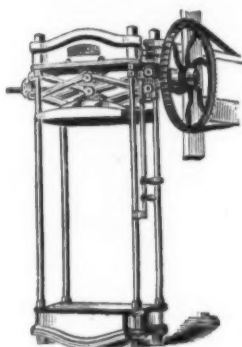
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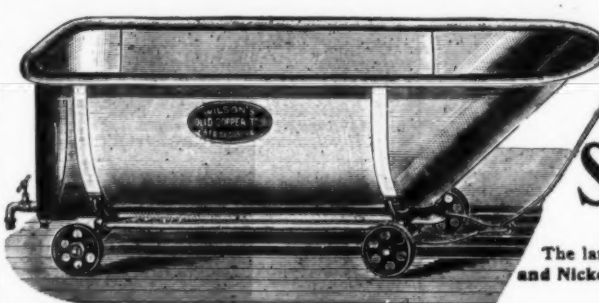
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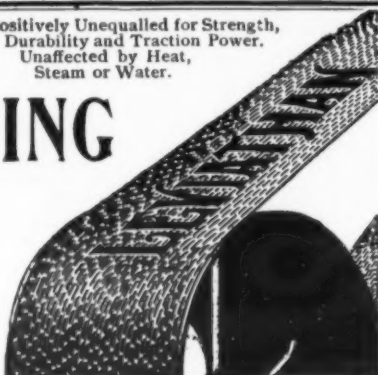
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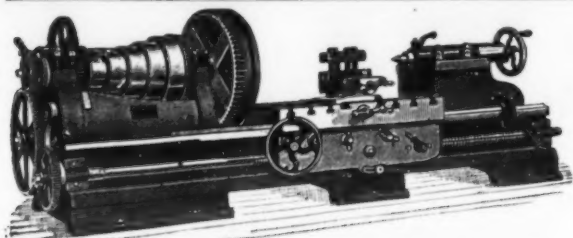
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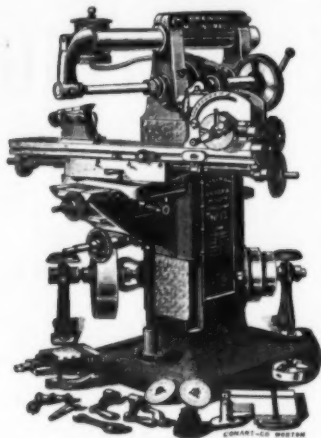
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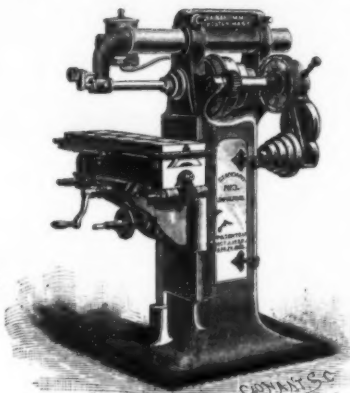


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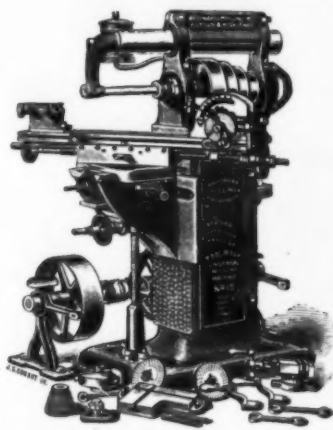
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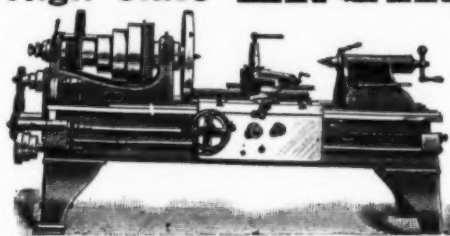
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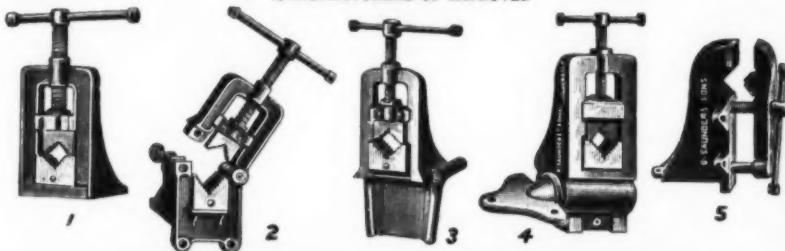
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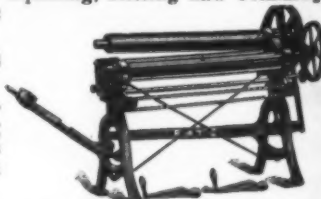


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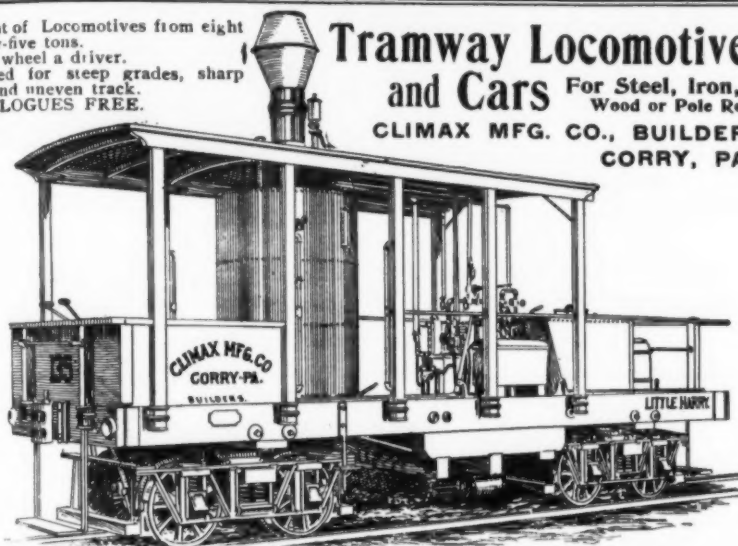
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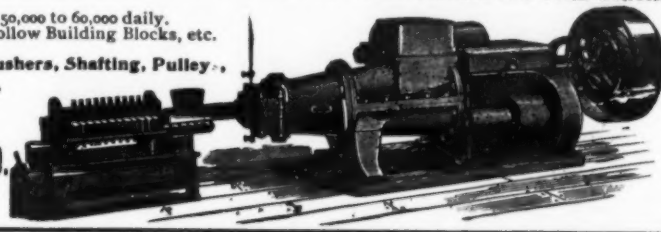
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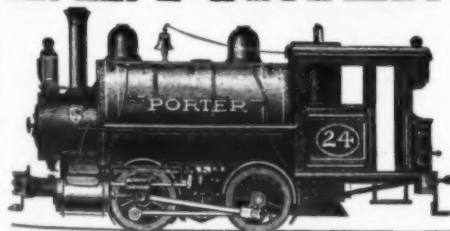


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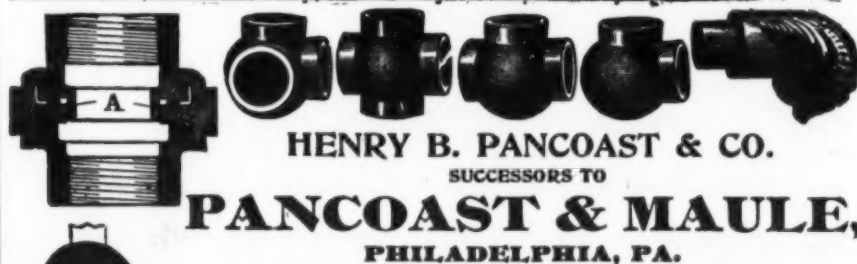
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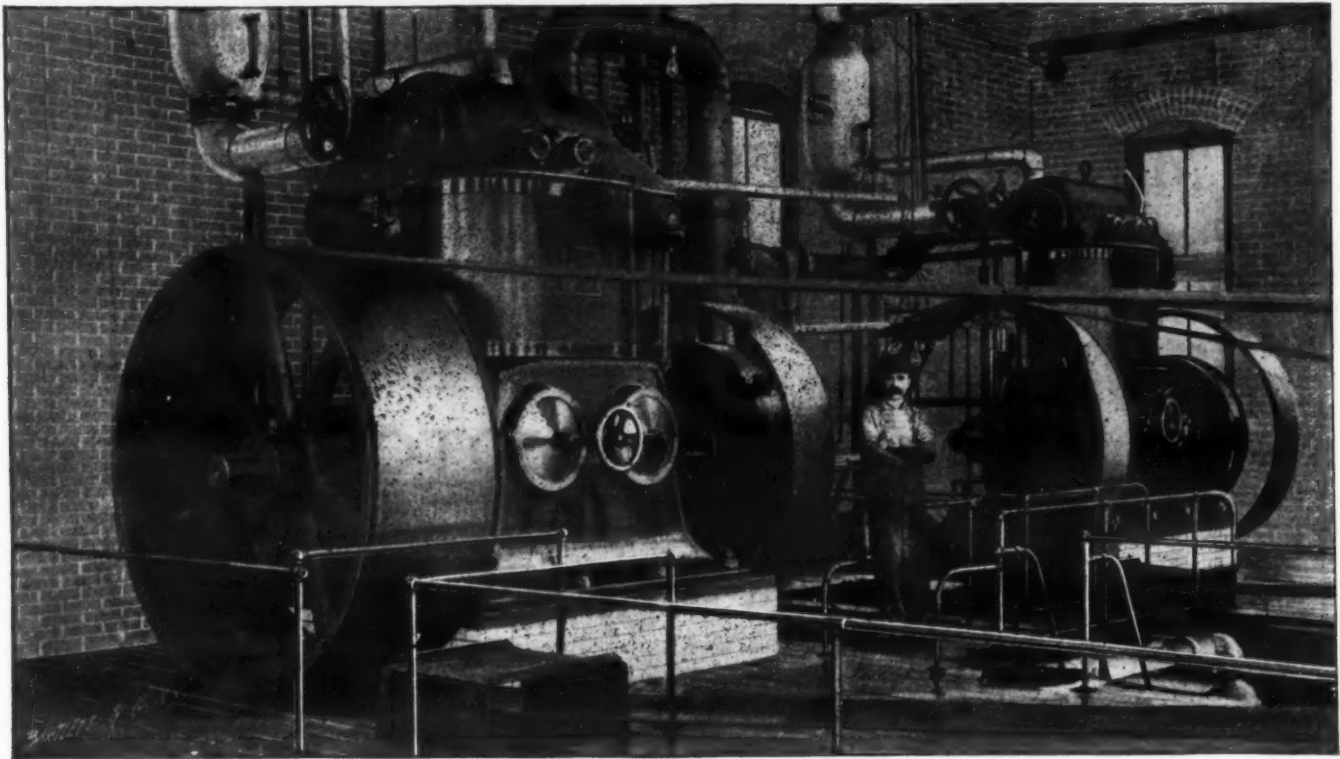
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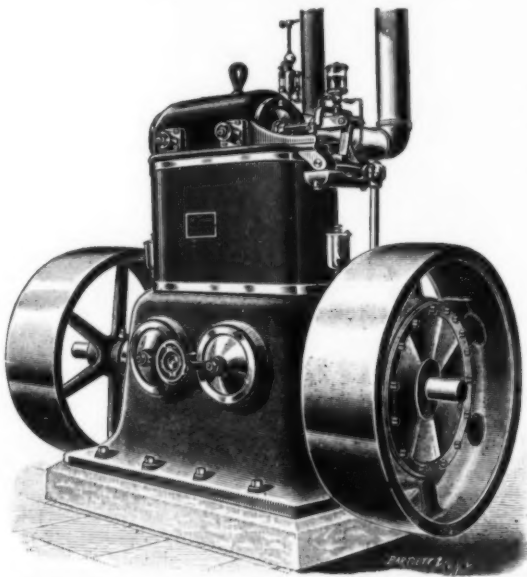
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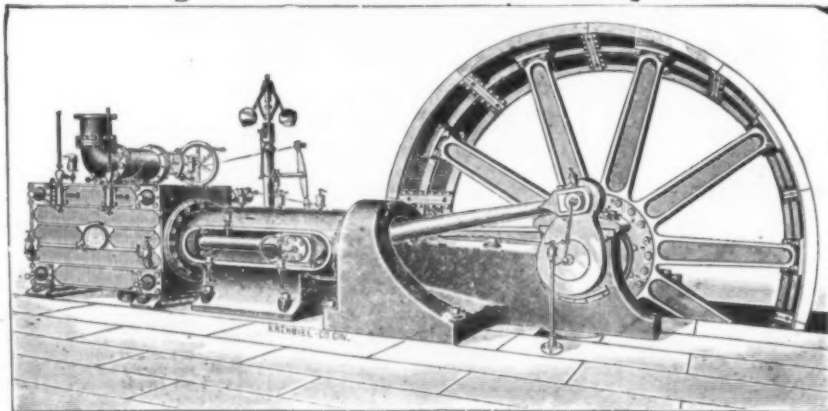
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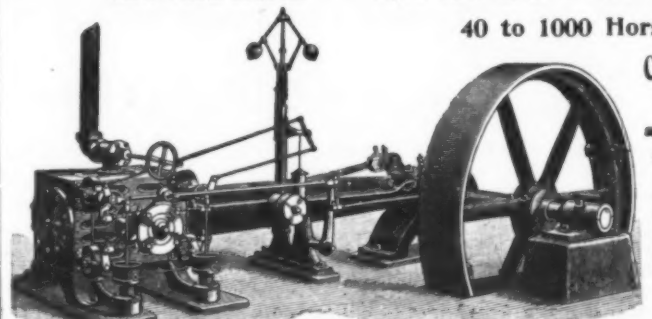
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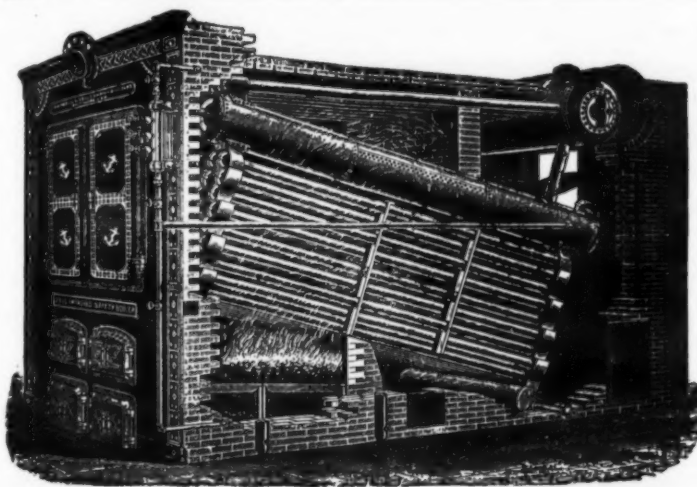
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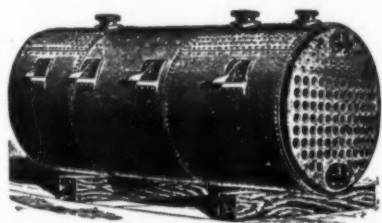
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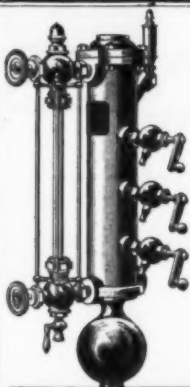
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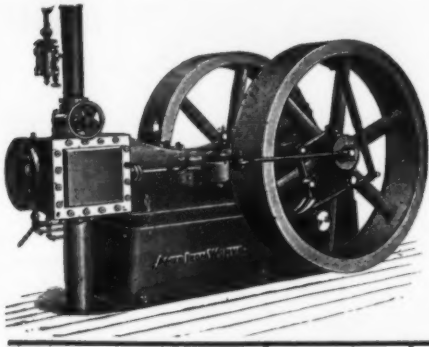
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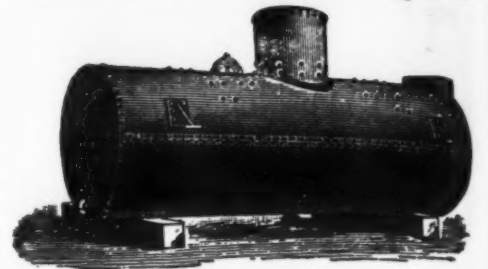
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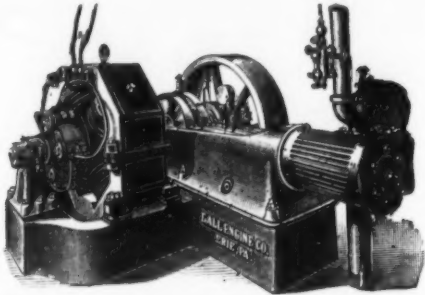
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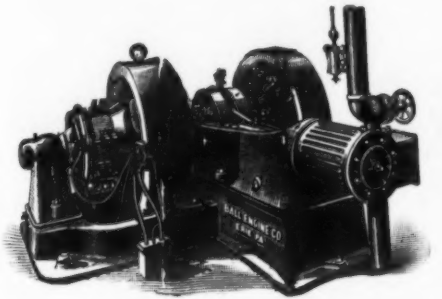
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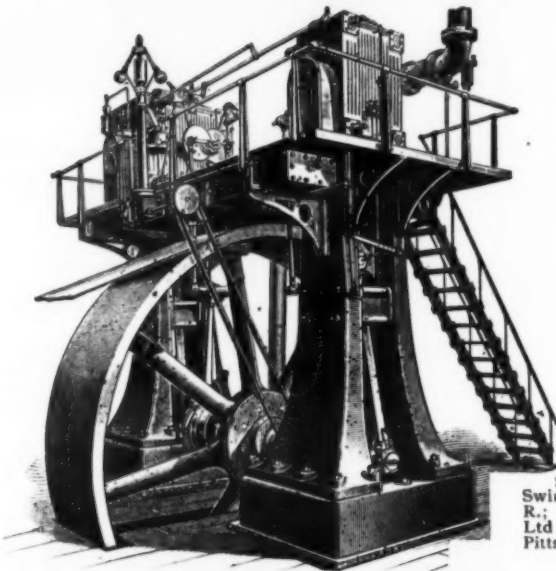
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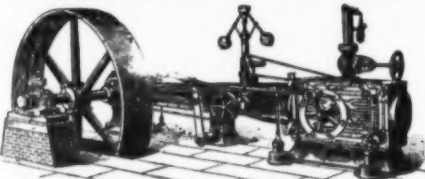
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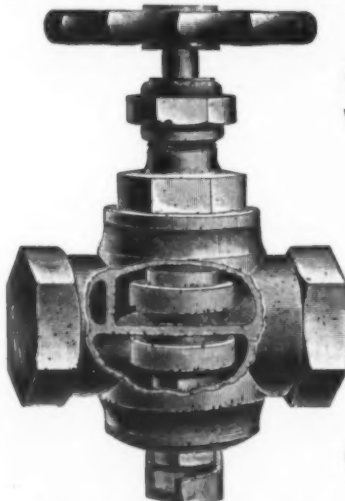
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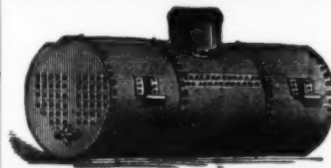
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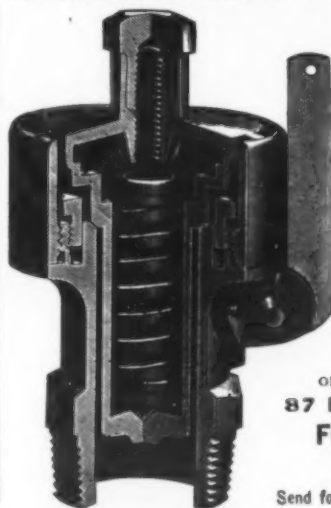
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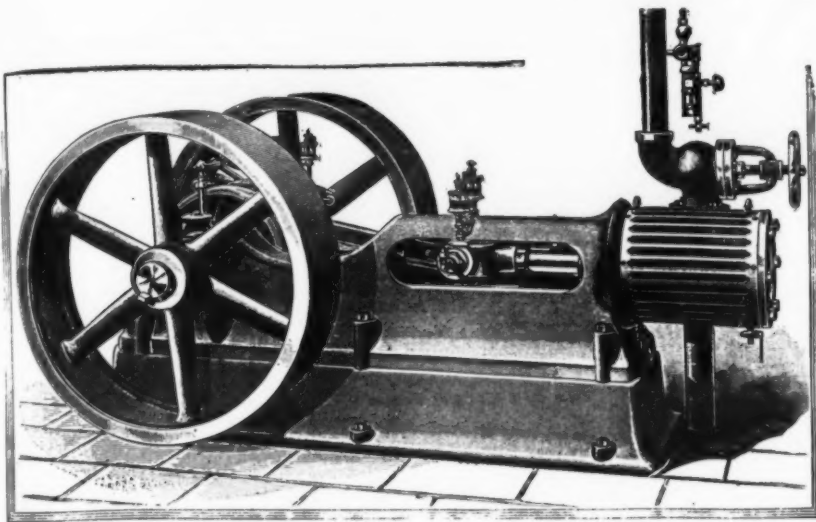
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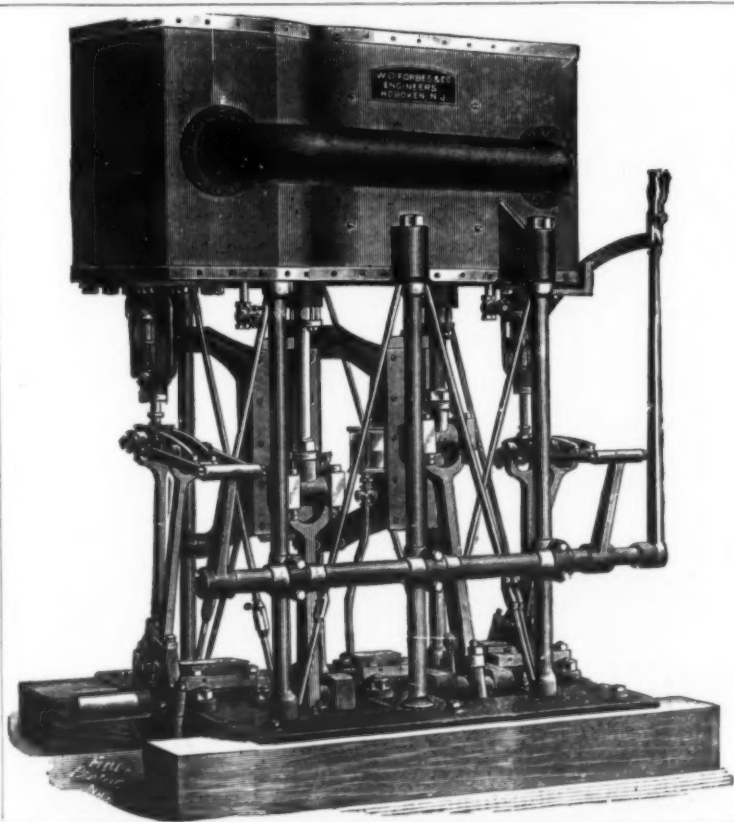
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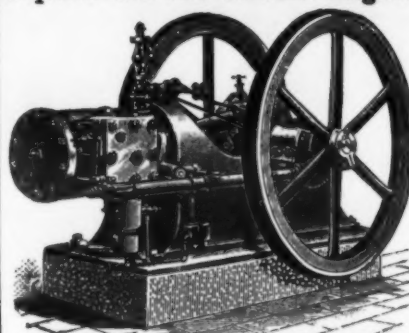
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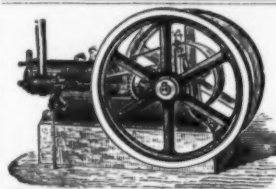


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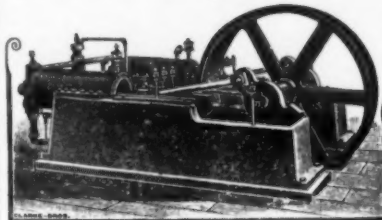


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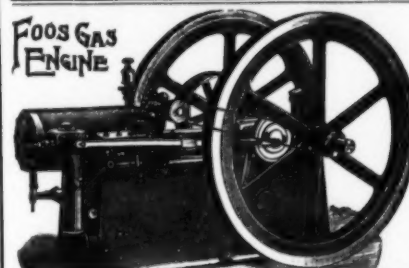


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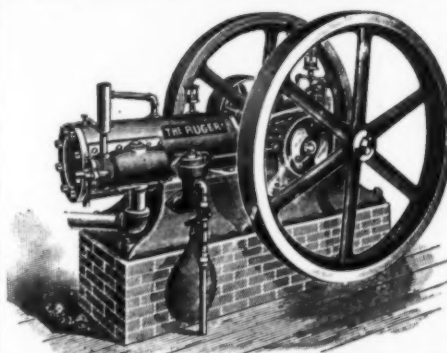
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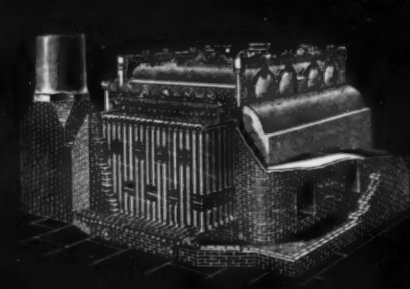
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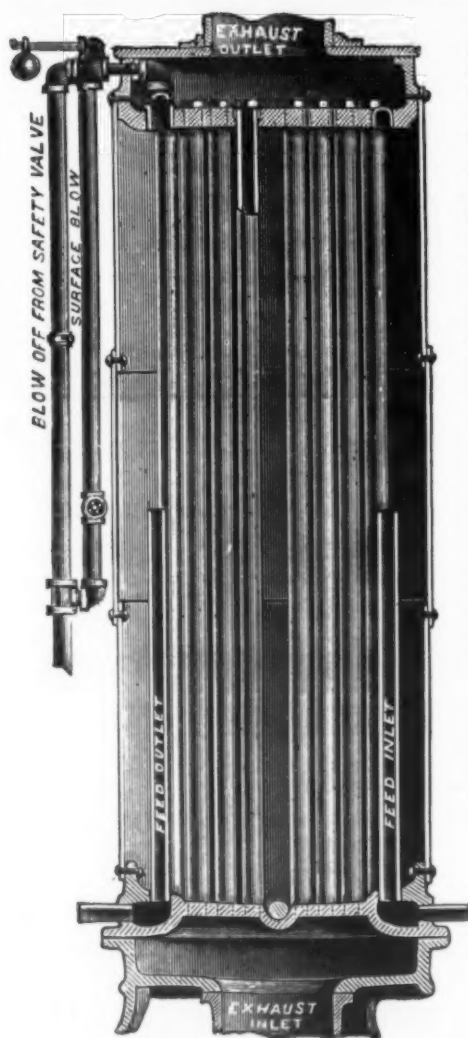
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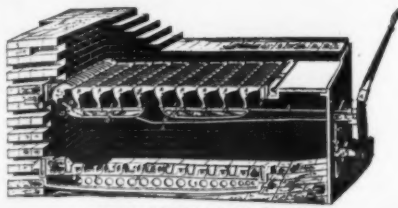
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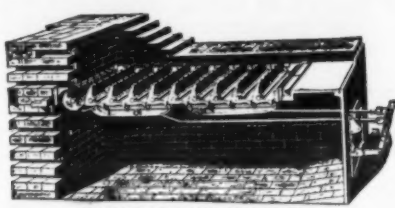
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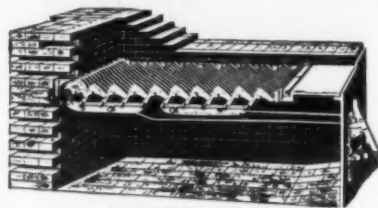
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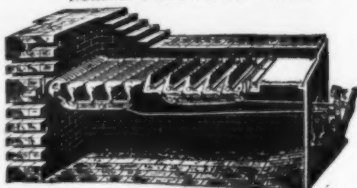
NORMAL POSITION OF GRATE.



WHOLE CUT-OFF MOVEMENT.



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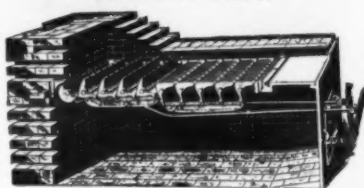


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These appliances together combine more valuable features for burning the smaller sizes of hard and soft coal, such as Anthracite, Culm, Birdseye, Buckwheat and Bituminous Slack, than any other system; while the Grate alone stands unequalled for burning the larger sizes of these fuels with natural draught. The Argand Blower gives excellent results in burning Spent Tan, Sawdust, Bagasse, &c.

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Gross Sectional View.



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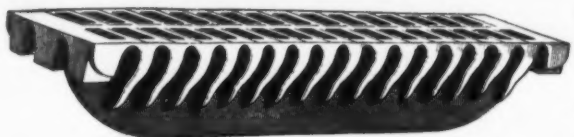
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A High Grade Tool that is never driven in nor out. The Bearings are Long, the Motion is Rotary, and the wear is evenly distributed. DOES BETTER WORK in less time than any other expander made. 2-inch EXPANDER \$12, delivered in U. S. All sizes in stock for prompt delivery. Write for prices. The JOHNSON TOOL CO., Wilkes Barre, Pa.



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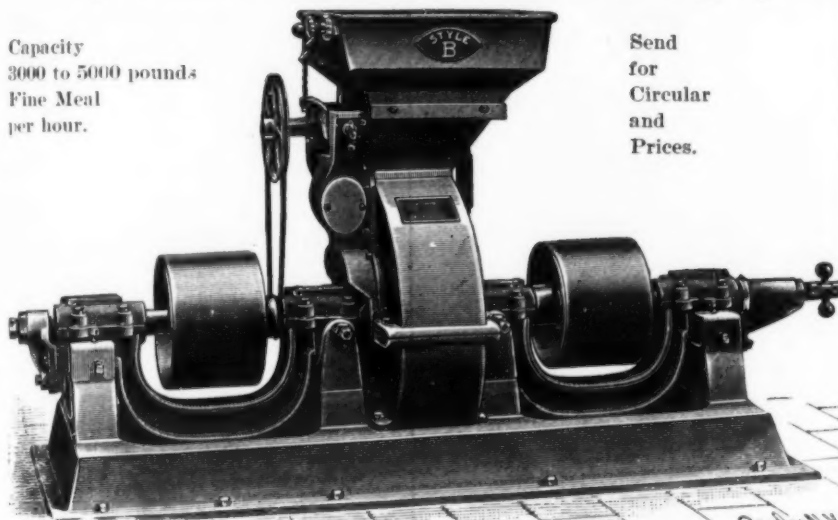
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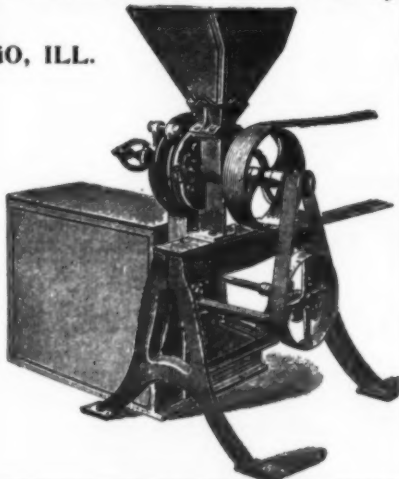
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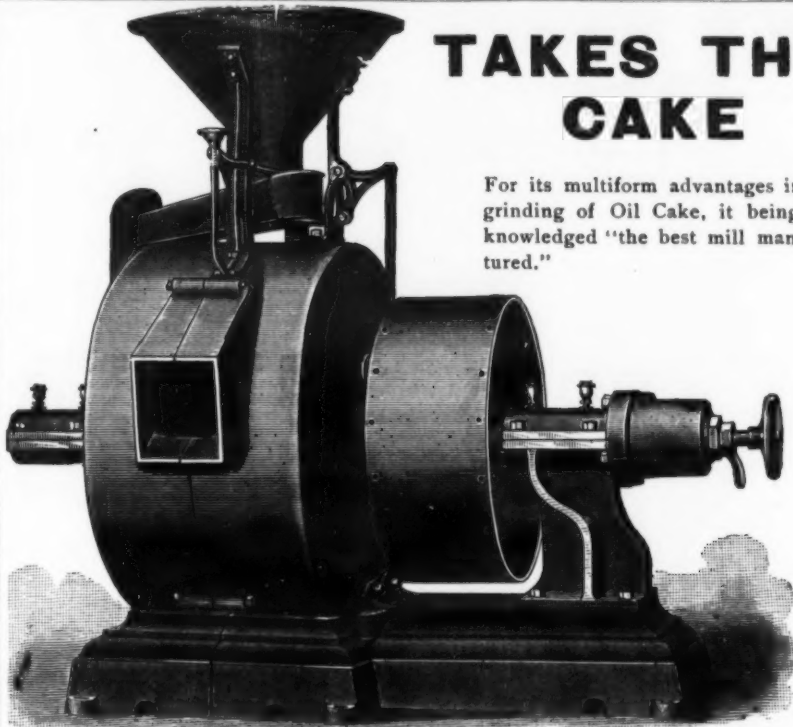
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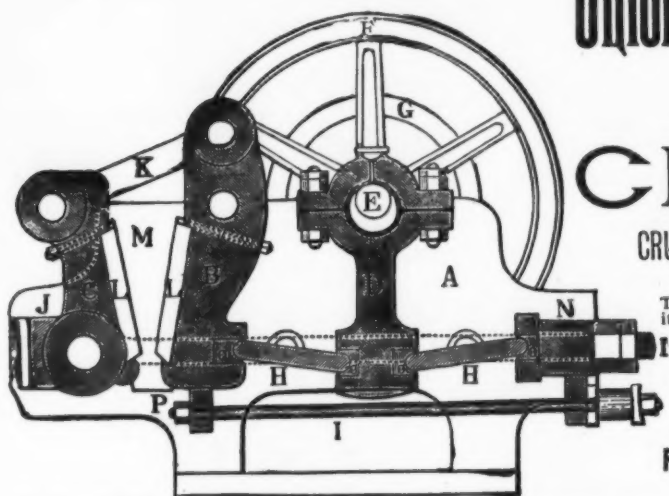
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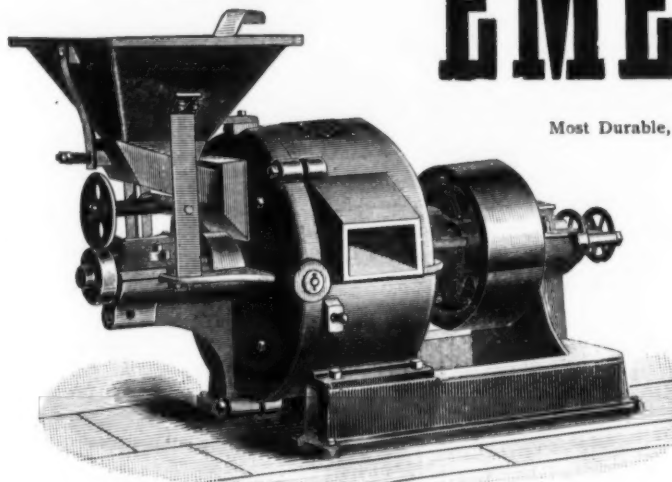
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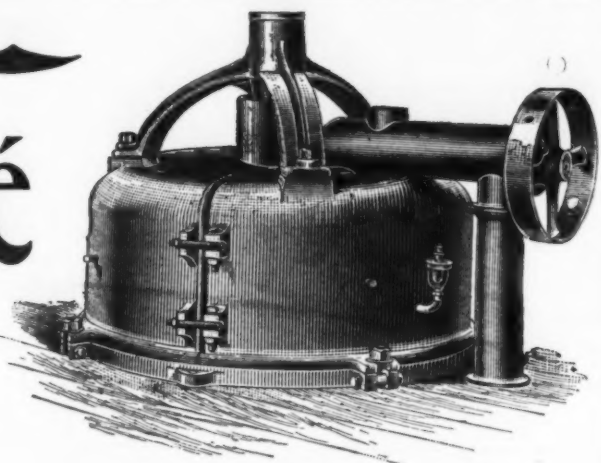
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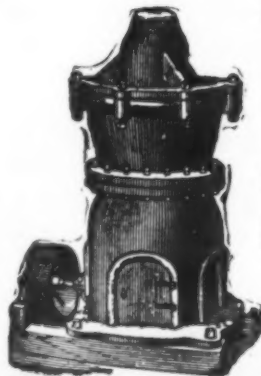
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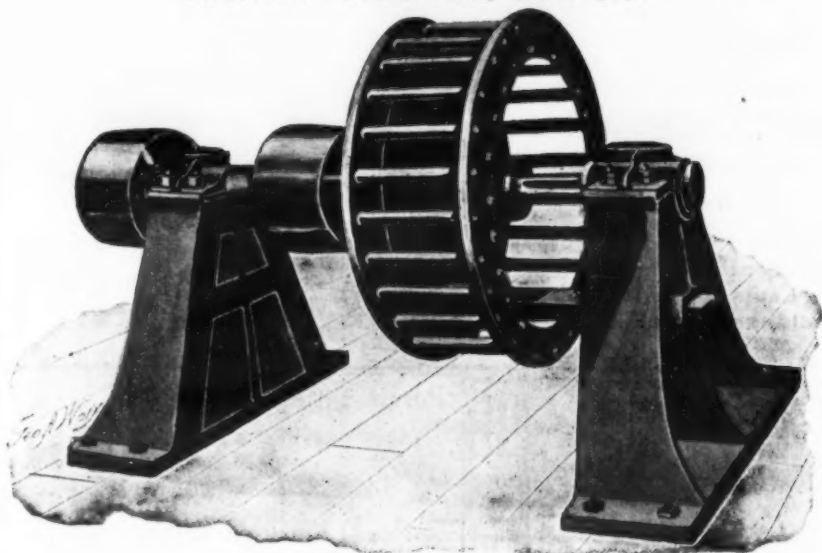
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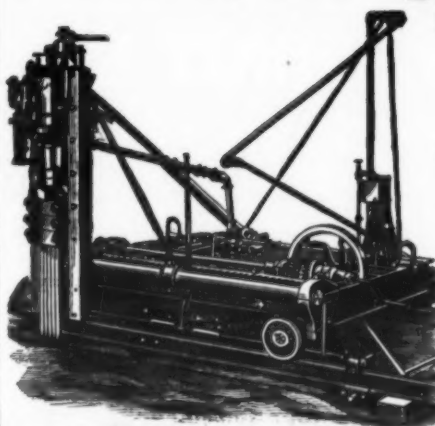
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Disintegrating Mill.

JAMES MURRAY & SON, 102 to 108 York Street, BALTIMORE, MD.

Sullivan Quarrying Machinery



SULLIVAN STEEL GANG CHANNELER.

Sullivan Channelers and Gadders,
Rock Drills and Quarrying Machinery,
Sullivan Prospecting Core Drills,
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STEEL GANG CHANNELERS,
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The New Sullivan Gadder has many points of advantage in capacity, convenience and economy.



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Class.	No.	Hands emp'd	Capital inv'd	Annual product.
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Bagging factories.....	1	325	300,000	350,000
Barrel factories.....	1	123	225,000	150,000
Basket and willow-ware factories.....	2	13	10,000	25,000
Basket & veneer wks.....	1	250	75,000	125,000
Breweries.....	1	100	100,000	300,000
Candy factories.....	10	100	100,000	350,000
Carriage & wagon wks.....	6	100	50,000	100,000
Cider and vinegar.....	3	15	15,000	35,000
Cigar factories.....	9	100	60,000	200,000
Cotton factories.....	1	645	\$600,000	\$700,000
Cotton & wool pick'g.....	1	15	10,000	30,000
Cotton ties, etc.....	4	20	10,000	25,000
Envelope & box fact'y.....	1	63	30,000	75,000
Fertilizer works.....	13	14	\$5,325,000	4,200,000
Flour and grist mills.....	8	275	150,000	700,000
Foundries & iron wks.....	3	200	250,000	300,000
Ice factories.....	3	25	75,000	100,000
Job printing, and book binding.....	8	180	325,000	450,000
Lead factories.....	1	10	40,000	130,000
Marble & granite wks.....	5	60	40,000	75,000
Mattress factories.....	2	35	27,000	70,000
Men's clothing.....	11	90	60,000	250,000
Oil refineries.....	1	5	20,000	35,000
Proprietary estab'nts.....	35	170	75,000	150,000
Rice mills.....	3	130	40,000	150,000
Sa. d'ries & harness.....	9	30	60,000	100,000
Sail & awning fact'ies.....	2	25	40,000	70,000
Sash & blind factories.....	5	320	350,000	850,000
Saw and lumber mills.....	8	250	70,000	850,000
Ship and boat yards.....	3	70	30,000	175,000
Soap and candle fact'y.....	1	15	25,000	60,000
Soda water, etc.....	22	150	70,000	650,000
Underwear factories.....	3	150	100,000	175,000
Totals.....	189	5474	\$9,590,000	\$14,272,000

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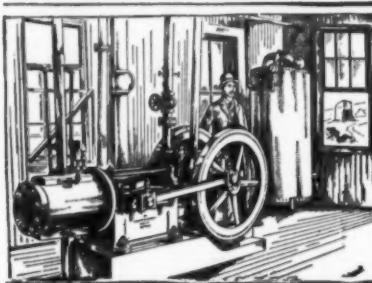
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BALTIMORE, OCTOBER 2, 1896.

One More for the South.

Even those closely following the business conditions prevailing in the South are somewhat surprised at the increase in bank clearings in the Southern cities, in face of a decrease shown in other parts of the country. The reports for the week ending September 26, as given by the Financial Chronicle, show an increase of 3.8 per cent. over the same period last year. The only other increase was in the Pacific coast group of cities, where the gain was merely nominal, but 1-100 per cent. The following table tells the story more forcibly than columns of description:

Middle States cities,	decrease 19.1 per cent.
New England cities,	decrease 18.9 per cent.
Middle West'n cities,	decrease 11.7 per cent.
Other West'n cities,	decrease 16.9 per cent.
Pacific cities,	increase 0.01 per cent.
Southern cities,	increase 3.8 per cent.

Grain Still Going South.

The heavy grain shipments from the States west of the Mississippi river to Gulf ports still continue, and, as might be imagined, railroad companies and shippers having interests in the Northern seaports are doing their utmost to divert the trade to its former exporting points. Freight rates are being changed, likewise elevator and terminal charges, and every inducement made to shippers to continue their business along the Eastern and Western trunk lines.

The latest advices from Kansas City are to the effect that freight agents of the Eastern lines are very despondent over the change of affairs, and have little hopes of again securing the business. In connection with this comes a dispatch from Mobile, Ala., to the effect that the grain consignments to that port are steadily increasing, and that a blockade is threatened on account of the scarcity of vessels to move it. The movement to Galveston and New Orleans also continues, and, as stated in the last issue of the Manufacturers' Record, two elevators are to be constructed at once at the former city to increase the storage capacity.

A Striking Interview with W. L. Trenholm, of New York.

The Manufacturers' Record publishes today a very striking interview with Hon. W. L. Trenholm, formerly of South Carolina, now president of the American Surety Co., of New York. Mr. Trenholm's statements were made in the course of a private conversation with Mr. Grasty, of the Manufacturers' Record's staff, but they were so important that they were written out and submitted to Mr. Trenholm with the request that he authorize their publication. In reply he wrote as follows:

I have read what you have written for the Manufacturers' Record, giving your impressions of our conversation of Saturday, and I take pleasure in saying that my views are substantially as therein described.

The inside history given by Mr. Trenholm of the first combination, made years ago, between Southern representatives and senators and the leaders of the silver movement in the Senate, presents a new phase to this question. Mr. Trenholm has done the South a great service in making clear the situation as to the past, and a still greater service in showing why it is no longer incumbent upon the men who made that combination to continue to support it, but why, on the contrary, duty and honor demand that they shall oppose free silver now. This is one of the most notable interviews of the present campaign.

The Value of Manufactures to Southern Farmers.

The value to the cotton planters of the South in having cotton mills at home is twofold. These mills create a home market for the cotton, and also a home market for all of the diversified products of the farm. The Jacksonville Times-Union, in making some comparisons of prices in different Southern cities, says:

The good price paid for cotton at Augusta is doubtless to a great degree due to the fact that that city is in a great cotton-manufacturing section, as well as a cotton-growing section. Probably 250,000 bales of cotton are manufactured within 100 miles of Augusta. This is fully half of the cotton that is grown in that radius. The mills in the cottonfields get their cotton for less than the New England or European mills have to pay, but it seems from the high price always paid for cotton in Augusta that the cotton-growers get a part of the amount that is saved from transportation charges by building the mills where they can get cotton direct from the growers.

These facts should be persistently impressed upon the people of the whole South. Some months ago the Manufacturers' Record published a letter showing that within a small circle around some of the large cotton mills of South Carolina the advance in the value of farms had been greater than the total capital invested in the mills. In its business life—and trade and commerce are civilization itself, for without business advancement civilization decays—the South's supreme effort should be given to the upbuilding

of its manufactures. Upon the growth of manufactures in the South is staked its whole future. Its agricultural interests, its schools, its churches, its banks, its merchants, the employment of its people, must all to a great extent rest upon this one thing. Develop its industrial life, diversify and advance its manufactures, utilize its vast natural resources, and every question that now perplexes this section will be settled. Immigration of well-to-do people, and of good farmers especially from the North and West, will bring a demand for farms, prosperity will come to the entire agricultural South, the race question will no longer trouble anyone, schools will be increased, and everywhere there will be seen a happy and prosperous people.

No Rubber Trees in Florida.

For several months statements have been published in many Northern and Western newspapers about the discovery in Southern Florida of extensive "caoutchouc" forests, and of the great opportunity they offered for profitable enterprise. These glowing accounts have possibly been furnished the press by one or more speculators, with the intention of interesting investors, preparatory to putting on the market a big scheme for purchasing lands and developing a new and very profitable Southern industry.

These stories have been told in an attractive manner, with what appears to be an honest intention to give conservative statements, and, in consequence, they have interested many people, who are seeking more definite information about the location, the extent of the alleged forests, the transportation facilities and various other matters about which prudent business men always ask before they give serious consideration to any proposed enterprise.

With the purpose of ascertaining how much foundation there might be for these stories, the Manufacturers' Record requested Mr. Wm. H. Martin, general manager of the Florida Homestead Co., Orlando, Fla., to furnish for publication whatever facts were within his knowledge. Mr. Martin applied to Reasoner Brothers, proprietors of the Royal Palm Nurseries, Oneco, Fla., for information, and by return mail received the following reply:

There are no "caoutchouc forests" in Florida, nor nearer than Brazil. The story emanated from a crank who came down here and saw three or four wild rubber trees. The overdrawn stories that have gone forth from his pen to the various papers are enough to make one distracted that knows the truth. There is but one rubber tree, *ficus aurea*. It grows very sparingly in this State on the keys and in extreme Southern Florida. It may produce a little inferior rubber, but is not worked at all. There are no rubber trees that can possibly be made to grow in large numbers and profitably in Florida. We have answered dozens of queries about this rubber fallacy, and hope the idea may soon die.

This reply ought to stop the rubber-bubble blowers. The nursery firm from whom it comes has probably as full

knowledge of the forestry of Florida as anyone possesses, and its statement ought to settle the question.

Mexican Cotton Mills Closed.

A dispatch from Queretaro, Mexico, says:

The Hercules Cotton Mills, of this city, which include three of the largest cotton factories in Mexico, closed down Saturday morning for an indefinite period. The closing of these mills throws 1600 operatives out of employment. The cause of the shut-down is stated to be on account of an overproduction of the manufactured goods.

According to the silver advocates, there is no overproduction in Mexico, business is prosperous and everybody is happy. But their statements and this dispatch do not harmonize.

Unequally-Placed Markets.

No one can deny that the South is naturally the best section of the country for agriculture. It is not cursed by uncertain seasons, and it is blessed with possibilities of greater diversity of crops than any other section offers. The one drawback to the agricultural classes in the South is the lack of a home market, and this drawback is disappearing with the growth of Southern cities. To show its effect on Southern agriculture, however, we will divide the Union into three groups, and give the population of each residing in cities or towns with 2500 or more inhabitants, and the population living in the country or smaller towns and villages. The first group embraces the States north of the Potomac and Ohio and east of the Mississippi, with Minnesota, which is partly on the east and partly on the west of the Mississippi. The second group consists of States south of the Potomac and Ohio and east of the Mississippi, with Louisiana, which lies on both sides of that river. The third group embraces the States and Territories west of the Mississippi. The following table shows the city and country population of each of these groups:

	Population.	
	Urban.	Rural.
Group 1.....	16,980,502	16,629,982
Group 2.....	2,029,902	12,934,100
Group 3.....	3,482,503	19,556,171
Total	22,501,907	49,120,253

It thus appears that in the Northern States east of the Mississippi, if the farmers produce twice as much as they use, they find a home market for it. In the South the farmers find a home market for only 14 per cent. of what they raise, while west of the Mississippi they find a home market for one-third as much as they use.

In fruits and vegetables, however, the South has the advantage of getting its products to the markets earlier than the Northern States, and thus for a time the South has a monopoly. Besides this, in the cotton crop the South has almost a monopoly of an important product, and if the growers would unite they could get any price they pleased. In addition to all this, the cities of the South are growing rapidly, and the home markets for Southern products are constantly growing larger.—Jacksonville Times-Union.

We believe that the Times-Union will admit that a home market would greatly increase the profitability of Southern agriculture, and that this home market can only be secured by the growth of a consuming population in towns and cities. This growth can only come through the development of manufactures, and this is best secured by a wise protective tariff. Hence the Southern farmer is really more interested in protection than anyone else.

A TRIBUTE AND APPEAL TO SOUTHERN HONESTY AND HONOR.

Col. W. L. Trenholm, the Great South Carolinian.
Makes Plain the Duty of the Hour.

THE DISTINCTION BETWEEN THE PAST AND PRESENT CLEARLY DEFINED.

Among the Southern men who have made homes in New York Col. W. L. Trenholm, formerly of South Carolina, who came into prominence as Comptroller of the Currency in Mr. Cleveland's first administration, occupies today probably the leading position as a financier. It has been said of him, that while he brought his head to New York, he left his heart down in Dixie. It happened that I caught Colonel Trenholm in his office on a Saturday afternoon, when the neighborhood of Wall street at this time of year is like a deserted village, and he talked to me as he would have talked to any other Southern man seeking his advice—like a friend and brother. There was no idea, either on his part or mine, that what he was saying would ever be put in print, else he might not have said all he did. But when he had finished, it came over me that here was such a complete refutation of the aspersions that have been cast upon our Southern people, such a tribute to their innate sense of honor, and such an appeal to their higher nature, to their patriotism, to their Anglo-Saxon honesty, and withal, such a clear-cut presentation of a history-making opportunity which will be theirs when the day of election comes, that I said as I left him, "Colonel, you owe it to yourself, you owe it to the South, and you owe it to the country to let me print what you have said." "If you can reproduce it from memory you may," was his response. His letter, which appears elsewhere, attests the substantial correctness of what follows.

THOMAS P. GRASTY.

THE SOUTH FOR YEARS WAS IN HONOR BOUND TO KEEP FAITH WITH THE FRIENDS OF SILVER.

"Though the parting of the ways has been reached," said Colonel Trenholm, "and the time is at hand when duty demands that the South shall turn from the supporters of free silver, yet those who are reviling this people—a people whose instincts are right, whose sense of honor is high, whose honesty of purpose is unimpeachable—seem to have forgotten, if they ever knew, how it came about that an alliance with the friends of silver was primarily a condition of Southern representation in Congress, and how afterwards allegiance to the friends of silver was demanded by the promptings alike of gratitude, loyalty and good faith.

A MATTER OF GRATITUDE.

"Until they can be brought to see this question in its true inwardness or to realize the situation in its new aspect, it is not to be expected that our people, taught from infancy to stand by their friends, will without good reason do anything that is made to appear to them like an abandonment of those through whose aid they obtained representation in the national government, and by whom, later on, they were rescued from what they believed would result in a subversion of their local governments.

THE PROPAGANDA OF ERROR.

"That this financial question, in its true inwardness, is not generally understood by them, is due partly to the influence of partisanship which is so strong with politicians, and partly to the propaganda so systematically maintained by the 'free-silverites' through the circulation of such literature as the plausible speech of Senator Jones, of Nevada, while there has been, until recently, no

adequate measures for a systematic campaign of education on the part of the gold people. These seem to have been willing to allow their side of the case to go by default, but now at the eleventh hour they are waking up to the peril brought upon the country by their overconfidence.

THE NEW ASPECT OF THE SITUATION.

"That the Southern people have not been brought to look at the situation in the aspect it has recently assumed is due largely to the same causes, but there is this difference between the situation then and now: Impending peril has become vividly manifest only since the Chicago convention gave, as far as it could do so, the sanction of the great democratic party to the revolutionary and visionary notions of men like Altgeld, Coxe and Jerry Simpson. It is now evident that the prevalence of a carefully-cultivated sentiment in favor of the use of silver money has been used as a rallying cry for all the dissatisfied, impracticable and thoroughly useless people of the country, who, in return for their support, have cajoled some of our venerable democratic leaders into a latter-day confession of populist faith.

COLONEL TRENHOLM RESENTS IMPROPER IMPUTATIONS.

"In a word, our people have not had time to recognize the dangerous and evil elements now disguised in the garb of 'regular' democracy. Therefore, when I hear any imputation of 'idiocy' or intentional dishonesty, or any intimation of a tendency toward repudiation or revolution on the part of these Southern people, who are quite as essentially American as those of any other section of the United States, and certainly not less manly or honest, I feel moved to indignation. Instead of condemning them, I feel that their leaning toward free silver, at least up to the manifestation of this new aspect of the situation—up to the time that there appeared these usurpers of democracy's heritage—will be justified by any truth-seeker who will look into this people's unparalleled political past.

HOW THIS ALLIANCE CAME ABOUT.

Let us see how it all came about. Four years after the passage of the 'Act of 1873' there came up to Washington for the first time after the surrender a large number of democratic Representatives-elect from the South, each having at his elbow a republican contestant, who had every reason to expect admission, because the republicans had control of the House of Representatives. They were disappointed. The democrats were seated. How? Mr. Randall made an alliance with the silver republicans.

"Thus while Mr. Randall had at heart only the interests of the democratic party, he builded better than he knew for the cause of silver.

"He enthroned his silver allies in the hearts of a people, who never did anything by halves. So that their minds were placed in a condition of fertile receptivity for the propagation of plausible errors—such, as I have said were embodied in documents like Senator Jones's speech, which have been industriously and indefatigably circulated from that day to this, not only in their original form, but in numberless repetitions in speeches by Southern Senators and Representatives.

A NEW OBLIGATION TO THE SOUTH'S FRIENDS IN AN HOUR OF PERIL.

"But for all that, the debt might have

been considered paid by the Southern votes in favor of the Bland-Allison act of 1878 and so-called Sherman act of 1890, had not a new obligation been incurred when that menace to local good government at the South, known as the 'Force Bill,' was thwarted through the co-operation of the silver Senators. The effect of this was not only to revive the alliance engineered by Mr. Randall, but to intensify it.

ONE OF THE OVERSIGHTS OF THIS CAMPAIGN.

"Now, let these historical facts be borne in mind. That they have been largely ignored by the advocates of sound money, has been one of the oversights of this campaign. There is no way of reaching a high-minded, chivalrous people but by frankness and candor. Their attitude must be considered from their point of view. And their point of view has been one of gratitude and loyalty to those who proved friends in need. But any people whose characteristics are thus noble may be relied upon to do right in the critical hour, no matter how trying the ordeal. The man who is capable of gratitude, and whose sense of honor is keenest, is always the man who, when he sees his duty, is surest to do it at any cost. Honorable men do not desert their allies until that course is demanded by principle. But the more honorable the man the more courageous and unflinching will be the stand he will take when principle is involved.

AN ILLUSTRATION OF THE EXACT SITUATION.

"Here is one way of illustrating the position in which the high-minded men of the South are placed today. Suppose that my house in Charleston had stood between an importer's wharf and his warehouse, and that it had happened that I was in a helpless condition and my family threatened from an attack from which there was no escape by the ordinary avenues of exit—an assault from which, if no way of escape had been provided, all were lost. And suppose that in this hour of dire perplexity the owner of this warehouse and of this wharf had come to me and said, 'Friend and neighbor, I realize your jeopardy, and have thought out a plan to save you. I find that a tunnel from my wharf to my warehouse under your residence will be useful to me, and at the same time provide your family with a way of escape when the spoilers make their attack.' I joyfully accept the suggestion. The tunnel is dug and my family is saved. My sense of obligation is profound. I feel that I can never repay my friend. But in course of time it comes to my knowledge that this man is no longer using this tunnel for legitimate purposes, but to smuggle merchandise and thereby defraud not only the government, but my other commercial friends, who pay duty on what they import. What now becomes my duty? Is it to allow my sense of obligation to this man to outweigh my principles and make me particeps criminis in transactions that my convictions tell me I should put a stop to at once? The ordeal is a trying one, but the question admits of but a single answer, and that immediately. I say to him, 'This nefarious use of this tunnel must be stopped, or I must close it up. No matter how great my debt of gratitude, you shall not make use of this tunnel, running through my land, to defraud your government to the injury of the business of your honest competitors. It hurts, but I have to tell you, plainly, quit this crookedness, or I close the tunnel.' But he relies so thoroughly on my gratitude that he doubts my willingness to do what I threaten, and refuses to abandon his evil work.

For me then to delay one hour in closing that tunnel, after I know its uses, would be a crime.

A MONSTROUS ABUSE OF GRATITUDE.

"For the silver-mine owners to appeal to the gratitude of this honest people, in order to induce them to perpetrate a fraud upon all the people—to work an irreparable wrong by demanding the misuse of a way of escape provided in an hour of peril—is simply monstrous. For the people of the South to hesitate, when asked to become parties to a crime, would be to do violence to these traditions, instincts and characteristics, and to the principles which have been their proudest heritage. Therefore, it is the duty of every man who can command their attention, or who enjoys their confidence, to make clear to this people just what is being asked of them—just what lies under the surface in this issue. It should be pointed out how the laudable promptings of a noble people are being worked upon for what is neither right nor wise, for what is neither honorable nor expedient. These are things that the politicians will not tell the people, if, indeed, they know or realize them.

"The editor of the paper with which you are connected (the Manufacturers' Record) has written an able pamphlet, showing in a convincing way that the act of '73' had nothing whatever to do with the existing general depression, and showing that free silver cannot benefit the Southern people. But I am not appealing to expediency. I am not going to put the duty of the South on the basis of mere self-interest.

THE ADVICE OF A CONSCIENTIOUS EXPERT.

"I could, by going into the merits of the financial question, show to the satisfaction of any man willing to be convinced that our financial troubles began with the act of Congress stopping the retirement of greenbacks, and how the locking up of all the silver that the Bland-Allison act and the Sherman act provided for the purchase of, has been worse in its effects upon the general prosperity than would have been the locking up of \$600,000,000 worth of wheat or cotton, on a falling market, instead of exporting it and getting the proceeds in money and credit. But I only have time to say to you on that score that it is my belief and my conviction, after studying this question diligently for years, that it would be an irreparable blunder, financially, for the South to vote for free silver.

THE HIGHER POINT OF VIEW.

"As I have stated, that is not the phase of the question uppermost in my mind. I am asking such of my fellow-countrymen as I am able to get in touch with to rise to a higher point of view; to look into this question as men of principle and as patriots, and not as partisans. I have no love for the republican party, whose accumulated burden of sins was unloaded on the hypnotized representatives of democracy at Chicago, but as a bounden duty, which patriotism and principle now stand hand in hand urging the performance of, I feel like pleading with every man that I know, and whose personal confidence I may enjoy, not to vote for Bryan. A Southern man may in this terrible ordeal even vote for McKinley and still maintain untarnished his sacred honor; but I believe the time will come when no Southern man will feel that he has maintained his honor untarnished, and his patriotism unsullied, and the honesty of his purposes unimpeachable, if, with a knowledge of the merits of this financial question and of what the Chicago platform really means, he shall have cast his vote for Bryan."

Government Expenditures for Harbor Improvements in the South.

The United States government has spent in the aggregate more than \$3,000,000 in improving the entrance to Charleston harbor. In a letter published in the News and Courier, of that city, Capt. F. V. Abbot, United States engineer in charge of the work, says that "but little more remains to be accomplished." The new jetty channel will serve every need. According to the latest survey, made in June, it has a straight and safe channel of eighteen and one-half feet at low water and of twenty-three and one-half feet at high water. The general depth is greater than this, and twenty-one feet at low and twenty-six feet at high water, the projected depth for "all the way through," has been attained, except for about 700 feet on the rapidly-wasting bar. "But little more remains to be accomplished," says Captain Abbot, adding that he has cash enough in hand to maintain the jetties and continue the dredging till the outer shoal is so far reduced as to cause no more inconvenience.

The fact that there are now but 700 feet of shoal between twenty-six feet of water, both outside and in the harbor, makes Charleston merchants happy, for in it they see the promise of recovering much of their former export trade.

Three million dollars is a large sum of money, but if it shall give to Charleston a permanent depth of water on her bar sufficient for all her commercial requirements, and so bring back to her the big cotton ships and great export trade that has largely been diverted to other ports, it will have been money well expended. All improvements of the harbors along our coasts that tend to increase the competition between interstate transportation lines, or to give to the people of a State or a group of States a first-class seaport, or that adds to the number of our harbors of refuge from storm and tempest, are incidentally beneficial to the city and State of their location, but, seen from a loftier standpoint, they are plainly so many valuable additions to the country's wealth and commercial importance.

If the statistics of the expenditures of the national government for the past twenty years on river and harbor improvements in the States from Maryland to Florida and from Florida to Texas, inclusive, could be given in detail and in aggregate, the total—even if all expended on the Mississippi and its tributaries were omitted—would amount to many millions, and of all this vast outlay these Southern States have, both directly and indirectly, had the immediate benefit, and no right-thinking citizen would have it otherwise.

There is one thing that the influential newspapers and prominent men of affairs in the South could do with good grace just now, that would inure to the well-being of their section and of the entire country.

While demagogues are shouting from every stump how the South has been robbed by legislation and kept in poverty by the manipulations of Wall street, and are filling the minds of their hearers with equally preposterous lies, the men and the editors that are well informed can tell their people quite another story, and, among other things, can tell of the work done in improving the Virginia, the Carolinas, the Georgia, the Florida and the several Gulf State harbors and rivers; of the millions this work has cost, and of the immediate and the ultimate advantages and benefits accruing to them and to their States because of these great expenditures by the general government. Most of their crude, mistaken notions about the government's relations to the people are due to the fact that their polit-

ical instructors for thirty years have sought to keep the South solid, and have therefore appealed to its passions, prejudices and selfishness, but never to its people's reason and common sense.

The Southern business men, who in the past have taken no active part in politics, are now awake to the dangers that threaten the country. * * * If Southern men will tell their fellow-citizens the exact truth about public affairs, there will be no longer a solid South, nor a divided, factional, quarreling South, but, instead, there will be a series of splendid Commonwealths, whose people's political acts will be governed by their convictions, and not by the mere say-so of self-seeking party leaders.—Worcester (Mass.) Spy.

The South Will Prosper.

[Baltimore Sun.]

With a settled financial policy and no fear of future agitation, the development of the South would progress with almost inconceivable rapidity, for no other part of the United States offers surer or more generous returns for investments. Despite the wails of the professional calamity howlers, its development, although retarded by the present unfavorable conditions, is yet so substantial as to excite the wonder of those who do not know the greatness of its resources, for, notwithstanding the statements of the croakers, the South is steadily adding to its wealth even under adverse circumstances.

A resident of Charleston, S. C., is quoted in yesterday's Sun as saying that within seven years the South will find it necessary to buy cotton to supply its mills unless the acreage is increased or the exports decreased. Less than a decade ago this statement would have been regarded as the rankest nonsense, but such has been the growth of the cotton-manufacturing industry in the cotton-growing States that no one can tell what proportions it will finally assume. It is evident, however, that the great mills in New England no longer find it profitable to compete with the Southern manufacturers in the production of coarse goods, and this branch of the business will soon be monopolized by the manufacturers whose plants are almost within sight of the cotton-fields. In good time the production of the finer grades of cotton cloth will be successfully accomplished by the Southern mills, and then the New England manufacturers will have to establish themselves in the South in order that they may reap the exceptional advantages which the mill-owners of that section enjoy. This will add vastly to the industrial activity and wealth of the Southern States, and will open up a field of development the extent of which cannot be overestimated. Not until confidence has been fully restored, however, can this influx of capital and energy be expected, and if the South only understood that its best interests depended upon its conservative and its emphatic repudiation of the vagaries and fallacies with which mischievous politicians are trying to allure it, there would be an immediate reaction in favor of a sound financial system.

The South is immeasurably rich in minerals, and abundant capital could be had to develop its natural resources if the voice of the political calamity howler were stilled forever and agitation was abandoned. That a section so blessed should offer a tempting field to investors is not surprising, and under those conditions which offer the proper safeguards to investments there will be a notable revival. With its immense stores of iron ore and its great deposits of coal, nothing is lacking to invite the manufacturer, and he will come to increase the general

prosperity when he is no longer menaced by such monstrous propositions as are advocated in the Chicago platform. It is a pity that a section to which the eyes of all who have funds to put into paying enterprises are now turned should have its development impeded by political agitation, for the South is the "coming" division of this mighty republic in many departments of activity, both industrial and agricultural, and its mines and its cotton-fields will yet be supplemented by hundreds of mills and furnaces in addition to those now successfully operated.

In spite of adverse conditions, however, the reports which come from that section are encouraging and indicate such an increase of prosperity as will make the South the centre of enormous activity. The planters have learned how to make a profit out of their cotton-fields, even with prices last year lower than ever before, and now that their staple crop has taken a substantial rise they will pocket millions of dollars as clear gain over the cost of production. Other departments of agriculture will also enjoy a revival when confidence has recovered from the rude shock of the past few years. There is no great amount of gnawing poverty in the South. Its people are in the main getting along well, according to the testimony of those who are thoroughly acquainted with the conditions there. And if they have emerged from a period of general depression with no greater injury to their material resources than has been represented by the best Southern authorities, they have been exceptionally fortunate, and may look forward to increased prosperity and unprecedented development if the conservative forces of the country triumph at the November election.

New Market for Southern Products.

Mr. Chas. E. Currie, who, as recently stated in the Manufacturers' Record, was appointed by the Louisville Board of Trade to visit Denmark and countries along the Baltic Sea, with a view of obtaining information for commercial purposes, has submitted his report. He states that fully 80 per cent. of the area of what is termed the Baltic country is devoted to agriculture, except Sweden, where considerable manufacturing is carried on. He says that there are a number of deep-water harbors which are open to commerce throughout the year, and that vessels of large size can enter and discharge cargoes without difficulty. He gives an elaborate description of the port of Copenhagen, where thirty feet of water is maintained at all times. Here are large warehouses and excellent facilities for transferring cargoes. Thus far Germany and England have enjoyed the bulk of the trade with the Baltic country, supplying about every variety of goods imported. A few varieties of American merchandise have been introduced into this market, among them machinery, leather, flour, shoes, harness and packing-house products. The people are favorable to American products, and, according to Mr. Currie's statements, the merchants would be pleased to handle them in large quantities. As an illustration, a New Orleans shoe-manufacturing firm, which opened an agency in Denmark three months ago, is now doing a business of \$5000 a month. In conclusion, Mr. Currie believes that the Southern merchants can find an excellent opening for trade in these countries.

A new paper, the News, has been started in Augusta, Ga., by H. P. Moore, John R. Burke and others. It is a morning paper, and will doubtless receive a liberal patronage if it is devoted to the best interests of the city.

RAILROAD NEWS.

[A complete record of all new railroad building in the South will be found in the Construction Department.]

Southern's Cotton Business.

The Southern Railway Co. will have its cotton compress at its terminus at Pinner's Point, Norfolk, in operation about October 1. The compress has been in use at West Point, Va., but much of the cotton which was shipped to that point will now come direct to Norfolk. Since September 1 the Southern has brought 22,623 bales to that city, out of a total of 55,179 bales.

To Ship Coal to Mobile.

Parties interested in making Mobile a great coal port have organized the Tuscaloosa Coal & Transportation Co. The incorporators include F. G. Blair, a mine owner, and Stephen Touart and T. W. Ruth, of Mobile. The company is capitalized at \$50,000, and is to employ a line of barges to carry coal from the mines above Tuscaloosa to Mobile. At first it will have a fleet of thirty barges in service.

A New Lumber Road.

An important short lumber road is now being completed in Columbus county, in the eastern part of North Carolina. It is being built by the Butters Lumber Co., which is operating at the town of Hub, near Chadbourn. The railroad, which is to be twelve miles long, will be used for hauling timber from Fair Bluff and vicinity to Hub, where the company has its mills located. Fair Bluff is also located on navigable water, and dressed lumber or logs can be shipped to the coast in tows or barges.

Fast Trains to Pacific Coast.

The Southern Pacific Company has made preparations to resume its vestibule train service direct between New Orleans and San Francisco, and it is announced that exceedingly fast time will be made by the Sunset Limited Express, as it is termed. This train will leave New Orleans every Monday and Thursday, reaching Los Angeles in thirty-eight hours and San Francisco in seventy-five hours from the time of starting. The trains will be made up of elegantly equipped Pullman cars, and will doubtless be as popular as last year.

Profit in Small Lines South.

As another indication of the profit in building small railroads in the South, the Auburn & Opelika Railroad, which extends between the towns named in Alabama, a distance of about seven miles, pays 8 per cent. annually on the entire investment of capital, after taking out all expenses and fixed charges. The Manufacturers' Record has a number of times referred to the business which has been done by these small lines, and there are plenty of opportunities for the construction of many others in localities where the operations would be just as profitable.

Cattle Shipments from Newport News

A dispatch from Newport News, Va., states that another steamship line is to be established between that city and Europe within a few weeks. The Thomson Line of vessels will be employed, and trips for the present will be made twice a month. The steamships are of large capacity, ten being in the present Thomson fleet. They have been plying between Montreal and European ports, carrying live-stock. It is expected to make a specialty of live-stock shipments also from Newport News, as well as general merchandise, cargoes being taken to Rotterdam and Amsterdam, Holland; also to Deptford,

England. The vessels are owned principally by Messrs. William Thomson & Sons, of Dundee, Scotland.

For Railroad Terminals.

A mortgage has been placed on file at Montgomery, Ala., which is of considerable significance. It is given by W. B. Strang, Jr., of New York, and J. W. Woolfolk, of Montgomery, well-known railroad promoters. The property covered by the mortgage is a tract of land excellently adapted for railway terminals, and it is understood that it will be used by the Montgomery, Tuscaloosa & St. Louis Company for depots and yards when this road is built. According to a dispatch from Montgomery, options have been obtained on \$70,000 worth of property for terminal purposes.

Illinois Central Improvements.

The Illinois Central Railway Co. continues to carry out the extensive improvements to its line, decided upon by the company some time ago. It has ordered sixteen heavy passenger locomotives, eight of which will be used on the branch between Louisville and Memphis, with a view to lessening the time of passenger trains. This division is also being rebalasted with stone and gravel and the track relaid with 75-pound steel rails, which will bring the road up to a high standard. The remarkable increase in traffic on the Illinois Central during the past year renders these improvements necessary.

A Florida Electric Project.

In a letter to the Manufacturers' Record, Frank E. Bond, cashier of the Volusia County Bank, of De Land, Fla., details an important enterprise which he is now promoting. This is an electric railroad line to extend from De Land to the Atlantic coast, through the towns of Daytona and Stetson. The road will be about twenty-six miles long, single track. Mr. Bond states that he has obtained exclusive franchise from the city council of De Land, and expects the same from Daytona, while the necessary capital has been promised to construct the road in the event of McKinley being elected in November. This will be the longest electric road in the State of Florida and one of the longest in the South when completed.

Norfolk & Western Offices.

The Norfolk & Western reorganization has been completed by the election of Henry Fink as president. It is understood that F. J. Kimball, former president of the company, will be elected chairman of the board of directors upon his return from Europe. William F. McDowell has been elected treasurer of the company; M. C. Jameson, controller; Jas. W. Coxe, auditor; E. J. Hemphill, secretary, and Joseph T. Davis, solicitor. The company has determined to transfer the executive and accounting offices from Philadelphia to Roanoke, Va., where a new office building is in the course of construction. It is understood that all the offices of the system will eventually be located at Roanoke, making it the headquarters of the company.

From Henrietta to the Gulf.

W. A. Squires, president of the Gulf & Brazos Valley Railroad, writes the Manufacturers' Record that this company has now been fully organized, with Mr. Squires as president; E. J. Gurley, of Waco, Texas, vice-president; C. B. Preston, of Mineral Wells, treasurer; H. M. Taylor, Mineral Wells, secretary and general manager; G. B. Gurley, engineer, and ex-Governor R. B. Hubbard, of

Tyler, general solicitor. It is intended to build this road from Henrietta, Texas, where it will connect with the St. Louis, Oklahoma & Texas Air Line Railroad, to Velasco on the Gulf, passing through the towns of Jacksboro, Mineral Wells and Waco. Mr. Squires has been interested in this project for some time, and has enlisted the co-operation of several capitalists in that State.

Will the Norfolk & Western Extend?

A dispatch from Knoxville, Tenn., states that with the completion of the reorganization of the Norfolk & Western system, the extension of this road to a connection with the Marietta & North Georgia will again be considered. At present the Norfolk & Western terminates at Bristol, Tenn., and must depend for a Southwestern and Southern outlet upon other lines. By building through Cocke and Sevier counties, in Tennessee, the connection with the Marietta & North Georgia road could be made, and the Norfolk & Western thus have an entrance into Atlanta, the terminus of the former line. As a proof of this assertion, it is stated that engineers have been making surveys from Bristol through the country referred to, apparently for a railroad line. If the extension is built, Atlanta will have three lines to points north of the Potomac.

Sale of the Reading System.

The sale of the Philadelphia & Reading Railroad system to J. P. Morgan & Co. is not surprising to those who have watched the manner in which this great Wall-street firm has become associated with railways in general. There are good reasons for supposing that Messrs. Morgan & Co. represent the Vanderbilts, and that the Reading is now in the practical control of the Vanderbilt family. This means that it will continue to be a competitor of the Pennsylvania system. The sale is of special interest to Southern people, from the fact that the Philadelphia & Reading is closely allied to the Baltimore & Ohio, forming a part of the "Royal Blue Line" service to New York. As the firm of Morgan & Co. reorganized the Southern Company, and will doubtless place the Baltimore & Ohio on a new financial basis, it is evident that step by step a combination of roads controlled by the Vanderbilts will form a direct route from New York to the heart of the South, verifying a prediction to this effect made in the Manufacturers' Record two years ago.

To Develop a Rich Country.

The Tredegar Mineral Railway, which, it is stated, is to be extended via Centre to the Chattanooga Southern Railway, is destined to become a most valuable piece of property, and it is more than probable that the mineral lands held by the company, which are the richest in the South, will shortly be developed. The main line of the Tredegar was projected from Jacksonville to Burke Iron Mountain, a distance of ten miles, and a branch was projected from Iron Mountain to Centre, twelve miles. A portion of the road from Jacksonville to Tredegar Junction has already been completed. Gen. Joseph W. Burke is at the head of this company, and since giving up the receivership of the Chattanooga Southern road has devoted his attention to the extension of the Tredegar and development of the ore mines.

Congressman Forney, who was once president of the company, together with General Burke and others, had the original charter ratified and additional powers granted by action of the legislature in February, 1893, giving the company power to hold or lease ore and mineral

and to do business in timber lands, stone, etc., in an enormously rich district.—Chattanooga Times.

Railways to Spartanburg.

According to a report from Spartanburg, S. C., several railroad projects of much benefit to that city may be carried out within the next year. A branch of the Southern Railroad is to be built from its main line to several cotton factories in the vicinity of the town, and surveys are now being made by the engineers of the Southern for this purpose. It is also stated that another company may be formed to build a railroad to connect Spartanburg with the Carolina Central division of the Seaboard Air Line. This company has no connection with the company which has been formed to build a road between Spartanburg and Henrietta. It is also stated that the electric line upon which work has begun between Spartanburg and the milling towns of Clifton and Glendale is to be completed after the November election. Considerable grading has already been done on its roadbed.

An Aid to the Lumber Industry.

The Gulf & Ship Island road, referred to several times recently in the Manufacturers' Record, will have an important effect in developing the lumber trade of Mississippi, where it is located. In a letter to the Manufacturers' Record, Mr. S. S. Bullis, of Gulfport, Miss., general manager of the road, writes as follows:

"The Gulf & Ship Island Railroad is now completed between Gulfport and Hattiesburg, a distance of seventy-one miles, the company has been reorganized and the receiver discharged. It is our intention to develop the business along the line to the greatest possible extent, especially as to its lumber resources, which it is expected will be by far the most important of any, as the road passes through a forest of yellow pine for sixty miles, as fine as any in the South.

"We have a pier off Ship Island harbor, where lumber can be transferred by barge to vessel, and have plenty of freshwater storage for export timber, where any quantity can be safely stored and towed direct to vessel by tug—the very best possible arrangement—and we also have direct connections and favorable traffic arrangements with the Louisville & Nashville and Queen & Crescent route for car shipments. We expect to make this road a lumber centre for the export trade especially. There are eleven saw mills now going up on the line, and as many are projected. Among others, the Christian-Craft Co., of Mobile, Ala., is interesting itself largely."

Railroad Notes.

Thomas D. Moore, of Cameron, Texas, has been appointed receiver of the Trinity, Cameron and Western road, now partially completed in Texas.

The Alabama Great Southern Railroad has determined to transfer its Southwestern offices to Houston, Texas. J. C. Andrews is in charge of this section.

The Dallas Terminal Railway has been placed in operation by the company owning it. The road reaches the principal factories in the suburbs of Dallas, Texas.

The Galveston, La Porte & Houston has placed an electric headlight on one of its locomotives for passenger service, and may equip others with the same light.

The Southern Express Co. has added the Paducah, Tennessee & Alabama road to its territory; also the Tennessee Midland. It will handle all the express business over these lines.

A dispatch from New Orleans states

that the Pullman Car Co. has decided to erect a building for its division offices in that city, and that plans will soon be prepared for this purpose.

W. Mason Smith has been elected president of the New Orleans & Western Company, and W. W. Pierce, vice-president. Mr. Smith succeeds Charles E. Levy, who has resigned on account of ill-health.

The steamship Lambert's Point recently left Galveston, Texas, drawing twenty-three and one-half feet of water. This is the deepest-draught vessel which has yet cleared from that port.

The Richmond Traction Co. has determined to increase its capital stock, and has ordered twenty-three closed cars for winter service. They are of the latest pattern, and combine all the modern improvements in electric car building.

The annual report of the Northern Central Railway Co. for August shows net earnings of \$144,631.58, compared with \$190,359.44 during 1895. For the eight months of this year, ending August 31, the net earnings were \$913,335.93.

The Hamburg-American Packet Co. has begun the operation of its service between Galveston and Hamburg, and the first steamer, the Scotia, has arrived at Galveston. It carried fifty passengers and a miscellaneous cargo. Most of the passengers were immigrants intending to locate in Texas.

A dispatch from Chattanooga, Tenn., states that the bondholders of the Chattanooga Electric Railway Co. have compromised with the company, the latter agreeing to pay \$18,000 interest money which is now due. This will probably result in the railway line remaining in the hands of the present company.

A dispatch from Mobile, Ala., states that fully 1,000,000 bushels of corn are about to be shipped from that port to Mexico, and that consignors are now looking for vessels to carry it. A large quantity of corn has already been sent to Mexico by way of Texas. This will greatly increase the already large grain shipments from Mobile.

The Southern Pacific Railroad Co. has issued a book descriptive of Mexico, which will be very valuable to anyone who desires to investigate the resources of that country. It is published by the passenger department of the road, and will doubtless be in great demand among prospective Mexican tourists. S. F. B. Morse is the general passenger agent of the company at New Orleans.

Many New Southern Offices.

The annual report of the Postoffice Department, regarding the changes in postoffices in the country, strikingly indicates the growth of the mail service in the South and the number of new offices rendered imperative by the increase in business and population. During the fiscal year ending June 30, 1896, 2046 additional offices were established in the country. Over 25 per cent. of these were located in the five Southern States of Georgia, Mississippi, Texas, Arkansas and Kentucky. The number was about three times as large as in the States of New York, Pennsylvania, Ohio, Indiana and Illinois, although these are five of the most densely populated Commonwealths of the country. In the State of Mississippi alone more offices were established than in the Dakotas, Montana and Washington combined.

Moore & Cumming, of New York, have secured the contract for dredging in Norfolk harbor. The price was eight and three-quarters cents per cubic foot. An appropriation of about \$90,000 has been provided for the purpose.

PHOSPHATES.

Phosphate Markets.

Office Manufacturers' Record,
Baltimore, Md., October 1.

The market for phosphate rock is a shade better, and manufacturers are more inclined to purchase. There is at best, however, a moderate volume of trade, but with the general improvement in fertilizer ingredients more activity is expected. In South Carolina there is a fairly active movement in phosphate rock, and the demand from domestic ports is better. There is also a good inquiry from foreign sources. Prices are nominally steady and unchanged. Crude rock at the mines is quoted at \$3, hot-air-dried \$3.25 f. o. b. Ashley river and \$3.45 f. o. b. Charleston city. Florida rock is firm, especially for pebble, and companies engaged in that section of the phosphate belt are limiting the output to the actual demand. In Tennessee there is considerable excitement caused by the discovery of phosphate beds at Mt. Pleasant, and capitalists are waiting for results of analysis of the product before investing. Several shipments from that section have already been made, and the work of development progressing rapidly. In the local market the following charters were taken this week: Schooners E. C. Ross and Vanleer Black to load phosphate at Ashley river for Baltimore. Freights in New York have been generally firm during the week, and shippers to distant ports are closing vessels suitable for their trade both for spot and forward loading. Tonnage for phosphate cargoes is scarce. Berth room is scarce, and rates firm. There were no phosphate charters reported during the week.

Fertilizer Ingredients.

The market at the close of the month of September shows more activity in certain departments, but owing to the uncertainty of political and financial matters, trade is somewhat restricted. There is a good inquiry for ammoniates from the South, and also from Eastern sources. Concentrated tankage f. o. b. Chicago is quoted \$1.20 to \$1.25; crushed tankage 9½ and 5, \$11.50; 9 and 20, \$13; 8 and 15, \$10, and 7 and 30, \$9, all f. o. b. Chicago. Ground blood f. o. b. Kansas City, Chicago and East St. Louis, \$1.30 to \$1.32½, and crushed f. o. b. Kansas City, \$1.25. Messrs. Thos. H. White & Co., in their September circular, say: "We have to record a fairly active business in the markets for ammoniates during September. The bidding from the South has been continuous, and supplemented in the last half by inquiries from several prominent Eastern manufacturers. The offering for September-October shipment has been liberal, but not sufficient to permit much, if any, advance in prices. The demand, however, has been largely for blood and high-grade material for November to February shipment. The fish catch has been fairly good, both in Chesapeake bay and on the coast. Recent sales were made at \$17 for dry." Nitrate of soda was rather easier in New York at \$1.75 for round lots on dock and in moderate demand.

The following table represents the prices current at this date:

Sulphate of ammonia, gas....	\$2 15@	—
Nitrate of soda.....	1 80@	1 85
Hoof meal.....	1 55@	—
Blood.....	—	1 60
Azotine (beef).....	—	1 60
Azotine (pork).....	—	1 65
Tankage (concentrated).....	1 50@	—
Tankage (9 and 20).....	1 40 and	10
Tankage (7 and 30).....	12 50@	13 50
Fish (dry).....	17 00@	17 50
Fish (acid).....	10 00@	11 00

Phosphate and Fertilizer Notes.

The British steamship Inverness arrived at Savannah on the 26th ult., consigned to Strachan & Co. She is char-

tered to load phosphate rock and other cargo for the Continent.

The steamship Rjukan sailed from Fernandina, Fla., on the 26th ult. for Stettin, Germany, with a full cargo of phosphate rock.

The Austrian steamer Aquilega, from Philadelphia, arrived at Port Tampa, Fla., on the 23d ult. to load a cargo of phosphate for a continental port.

The British steamship St. Filians was cleared from Brunswick, Ga., last week by the Brunswick Terminal Co. for Liverpool and Manchester, having among her cargo 1000 tons of phosphate rock.

The British steamship Elwick took out last week among her cargo from Savannah for Genoa, Italy, 1194 tons of phosphate rock, and the British steamship Sidra, for Hamburg and Bremen, 1515 tons of phosphate rock for the latter port.

The British steamship Abarenda was cleared for Bremen last week from Savannah, Ga., by Wilder & Co. She took out 1810 tons of phosphate rock and other cargo. The British steamship Brinkburn cleared on the 23d from the same port for Hamburg having 1796 tons of phosphate among her cargo.

The exports coastwise of phosphate rock from the port of Charleston, S. C., for the week ending September 26 were as follows: Schooners Bertha F. Walker for Elizabethport, N. J., 1615 tons; Mary Curtis for Richmond, Va., 600 tons; S. B. Martz for Baltimore, 800 tons; Rebecca J. Moulton for Weymouth, Mass., 800 tons acid phosphate. The total shipments of phosphate rock from Charleston from September 1 to 26, inclusive, amount to 8815 tons domestic and 1200 tons foreign, against 4864 tons domestic for same period last year.

Iron Markets.

Cincinnati, Ohio, September 26.

The revival of confidence is becoming epidemic, and is no longer confined to isolated spots. Everyone and all sections are beginning to feel that the period of apprehension and fear has gone by, and that preparations should be made for a return to normal conditions. The newspapers are reflecting the sentiment, and it is apparently but a question of comparatively short time before the great wheels of industry will be revolving with their old-time force.

Pig iron is generally one of the first articles to feel the change in business conditions, and certain premonitory signs have appeared that give hope to those interested.

Less than a month ago the consumers of iron said: "Let the furnaces go ahead and sell their surplus stocks to investors if they desire. There will be plenty left for us, and at lower prices." Furnaces have continued to blow out, production has constantly decreased, and a bottom in prices has been reached, lower than which none could go and keep the sheriff away. Another Tennessee furnace stopped this week. There is today a greater readiness to buy at the quotations, which are up 50 cents per ton, than when they were at the bottom. If sanguine prophets are to be believed, there will be no long pause on this plane, for it seems reasonable that the few producers able to operate should only sell at a fair margin. They are not more than keeping even now, and some are still losing. One of the prominent Alabama companies has announced its determination not to sell except at 25 cents per ton further advance.

The past week has resulted in considerable tonnage in the Central West. Several round lots of Northern and Southern irons have been bought by implement and machinery men, and a number of other deals of importance are under way.

Reports continue to be published of greater activity among mills and foundries, but personal investigation over the entire iron-consuming portion of the country shows little enlargement of consumption. But it will come in due time.

We quote for cash f. o. b. Cincinnati:

Southern coke No. 1 foundry.....	\$10 00@	\$10 25
Southern coke No. 2 foundry.....	—	9 75
Southern coke No. 3 foundry.....	—	9 25
Southern coke, gray forge.....	—	9 00
Southern coke, mottled.....	—	9 00
Southern coke No. 1 soft.....	—	9 75
Southern coke No. 2 soft.....	9 50@	9 75
Lake Superior coke No. 1.....	12 00@	12 50
Lake Superior coke No. 2.....	11 50@	12 00
Hanging Rock charcoal No. 1.....	15 50@	16 50
Tennessee charcoal No. 1.....	13 00@	14 00
Jackson Co. silvery No. 1.....	13 50@	14 00
Standard Alabama car-wheel.....	14 75@	15 50
Tennessee car-wheel.....	13 50@	14 50
Lake Sup. car-wheel & mail'e.....	14 75@	15 25

St. Louis, Mo., September 26.

Since the advance in Southern pig iron the market has been awaiting future developments. Some sales to cover immediate requirements have been made during the past week at advanced prices.

There is a slight improvement in general foundry trade.

We quote for cash f. o. b. St. Louis:

Southern coke No. 1.....	—	\$10 75
Southern coke No. 2.....	—	10 25
Southern coke No. 3.....	—	9 75
Southern gray forge.....	—	9 50
Tennessee charcoal No. 1.....	—	12 00
Lake Superior coke No. 1.....	—	14 00
Lake Superior car-wheel.....	—	15 25
Southern car-wheel.....	—	15 25
Genuine Connellsville coke.....	—	5 00
West Virginia coke.....	—	4 50

Chicago, Ill., September 26.

The activity displayed in Eastern iron markets during the past week is not equalled in the Western States. The heavy tonnage booked by Eastern sellers has rendered Southern furnaces comparatively comfortable, so that they are not disposed to shade their established figures even to meet competition of local irons in this market. The latter is now an important factor, as a number of the larger consumers are able to regulate their mixtures so as to use a larger proportion of the cheaper iron, whether it be Northern or Southern metal. Southern producers are holding prices firm at the recent advance, although it is rumored that sales have been made within the past few days at something lower.

The general aspect of things is more encouraging. Shipments are moving more freely, and while the sales being booked are not very large, inquiry is increasing, and buyers are showing more disposition to take hold.

We quote for cash f. o. b. Chicago as follows:

Lake Superior coke No. 1 fdy.....	—	\$11 50
Lake Superior coke No. 2 fdy.....	—	11 00
Lake Sup. charcoal, Nos. 1 to 6.....	13 50@	14 50
Ohio Scotch No. 1.....	14 00@	15 00
Jackson Co., O., silvery No. 1.....	14 50@	15 50
Alabama silvery No. 1.....	—	11 85
Southern coke No. 1 foundry.....	—	11 50
Southern coke No. 2.....	—	11 00
Southern coke No. 3.....	—	10 50
Southern coke No. 1 S.....	—	11 00
Southern coke No. 2 S.....	—	10 75
Mannle.....	11 75@	12 50

Buffalo, N. Y., September 26.

The market in this district has shown more life this week than for some time past. The new orders entered have been from the smaller consumers, but for immediate shipment. Many of the larger consumers have asked for quick shipment on existing contracts, which goes to show that stocks in the foundry-yards are light, and the consumption increasing in a small way.

Several sales of Lake Superior charcoal have been reported to concerns who are stocking up for their winter's consumption.

We quote, on a cash basis, f. o. b. cars Buffalo, as follows:

No. 1 foundry strong coke iron.....	—	\$12 50
Lake Superior ore.....	—	12 00
No. 2 foundry strong coke iron.....	—	12 50
Lake Superior ore.....	—	12 00
Ohio strong softener No. 1.....	—	12 00
Jackson county silvery No. 1.....	—	15 25
Southern soft No. 1.....	—	11 40
Southern soft No. 2.....	—	11 40
Lake Superior charcoal.....	14 00@	14 50

New York, N. Y., September 26.

A slow but genuine improvement in

financial conditions is starting up foundries and mills, in a small way at first, but in a manner to indicate that better things are coming along later. Men are more cheerful and hopeful than they have been since the Chicago convention assembled. Naturally the effect is felt in pig iron in larger orders and better prices. Both Northern and Southern furnaces are taking about all the business they care to at ruling figures. No more is heard of the incredible stories of cut prices that were circulated in July and August. The strongest bear argument advanced this week was that Bryan was coming, and his three speeches here would knock 50 cents off pig iron. He has come, spoken and gone, and the markets are stronger than before. His second advent made scarcely a ripple on the surface of the metropolis. This was in significant contrast with the first, which stirred up the waters from the bottom.

Banks are buying paper this week for the first time since July, but the amounts are small and the rates high. Decidedly lower rates are probable in October if the gold-import movement continues.

We quote for cash f. o. b. docks or cars New York:

No. 1 X standard Southern.....	—	\$11 25
No. 1 X choice Virginia, such as Shenandoah.....	—	12 00
No. 2 X Alabama or Virginia.....	—	10 75
No. 1 soft Ala. or Virginia.....	—	10 75
No. 1 X lake ore coke iron.....	—	14 00
No. 2 X lake ore coke iron.....	—	13 50
Lake Superior charcoal.....	15 85@	16 35

Philadelphia, Pa., September 26.

There is no excitement in the pig-iron market here, but sellers are holding standard brands very firmly, and buyers are placing orders for nearby requirements. Iron is coming forward liberally from Alabama, and the rail and water lines are pushed to the extent of their ability.

Light stocks in consumers' hands are reported, and a steady demand can be expected during the remainder of the year.

We quote for cash f. o. b. Philadelphia docks:

No. 1 X standard Alabama.....	—	\$11 25
No. 2 X standard Alabama.....	—	10 75
No. 1 X standard Virginia.....	—	11 75
No. 2 X standard Virginia.....	—	11 25
No. 1 X lake ore iron.....	—	14 00
No. 2 X lake ore iron.....	—	13 50
Lake Superior charcoal.....	—	15 85
Standard Georgia charcoal.....	15 25@	17 00

ROGERS, BROWN & CO.

Want Names of Pipe and Pig-Iron Manufacturers.

J. Anderson jun. & Co., Copenhagen, Denmark, in a letter forwarding their subscription to the Manufacturers' Record, say:

"We take the liberty of requesting you to kindly name for us the addresses of the highest-reputed manufacturers of wrought-iron pipe, cast-iron pipe and pig iron in America."

The Danish Market.

Mr. Charles E. Currie, who was sent by the Commercial Club of Louisville, Ky., to investigate the trade outlook in the countries bordering the Baltic sea, and whose report was referred to recently in the Manufacturers' Record, has been appointed Danish vice-consul at Louisville. In a letter to the Manufacturers' Record, Mr. Currie states that he has taken the office simply to develop and encourage trade with that country, and will be pleased to correspond with anyone interested in the matter.

About seventy-five students are now attending the Rolla (Mo.) School of Mines, which is reported to be in a very prosperous condition. This is one of the most valuable institutions in the Southwest, and is accomplishing much in an educational sense.

COTTONSEED OIL.

This department is open for the full and free discussion of trade topics and practical questions, and contributions are invited from men who are identified with this industry. Items of news are always acceptable.

Can Cotton-Oil Mills Profitably Crush Flaxseed?

It was my pleasure recently to forward the Manufacturers' Record an article pointing out the feasibility of manufacturing linseed oil in cotton-oil mills. In an article which appeared in last issue exception is made to the correctness of same, and in a manner which is eminently inconsistent, when it is considered that the objections cited emanate from one who assumes a thorough knowledge of oil-milling conditions, North and South.

At this especial time I am occupied on a task which absorbs every available hour; nevertheless, for the dissemination of useful knowledge and coincidentally reply to my critic, will go over the ground again, briefly as possible.

In the first place, I don't "urge" anything on the cotton-oil men in the way of a new departure. I have merely submitted my views on the practicability of making linseed oil in Southern cotton-oil mills, basing same on my personal experience.

Notwithstanding that I am as sincere in my desire to discover new outlets for cotton oil and expand existing ones as the most ardent Southerner could wish, the fact is undeniable that the unctuous fluid extracted from cottonseed is an unsatisfactory paint oil under any conditions, and cannot compare in this respect with linseed oil. This is nothing new. I presume everybody knows it. Now I know that a fair volume of linseed oil is shipped from the East and West to the South, as well as to Mexico, annually. Anyone can ascertain the fact. The price charged in the South for linseed oil, even in large cities, such as New Orleans, is excessive, when compared with those quoted in the producing locations.

Here permit me to say I have for years experimented, in conjunction with the most eminent industrial chemists in the United States, the object being to render cotton oil, by chemical treatment, a good substitute for linseed oil, or rather to entirely supplant the use of the latter and create a new and illimitable outlet for this interesting product of the South. Needless to say, all efforts were unavailing. Untold wealth awaits him who accomplishes this desideratum. As a drying oil cotton oil is a failure, and all the sophistry which may be uttered or printed cannot alter the fact.

Evidently there is a demand for linseed oil in the South to a greater or less extent. I will now say that this oil, as well as every gallon which passes overland to Mexico, or by other means of transit, should be made right in the South. I will take hold of any mill in the South, large or small, and make linseed oil equally as well as it is made in the North, without making a change in the plant construction. I have made oil from linseed, cottonseed, rapeseed and poppy-seed with the same presses regularly; absolutely no change in system, excepting the necessary regulation of heat and moisture in the cooking kettles. Today there are mills running on linseed and castor-seed in the United States, and I am in receipt of letters from Southern cotton-oil men—practical oil millers—concerning the practicability of crushing castor-seed alternately with cottonseed. No good reason can be advanced why this cannot be done; the system is in actual operation in France and England. The question of crushing and extracting

oil from sunflower seed and peanuts is also receiving consideration, as is well known.

Mr. Johnson says: "Linseed oil is absolutely unfit for any edible use whatever." I have made hundreds of barrels of linseed oil annually which were used exclusively by the natives of Northern Africa as an edible product, just as cotton oil is used now. Edible cotton oil, I am happy to say, is consumed by more refined palates, and it is a noteworthy fact that it has almost entirely driven the linseed oil out of the market referred to, its comparative cheapness being the prime factor in effecting the change. Furthermore, all the cotton oil made is not consumed for edible purposes, and it is quite practicable to run a section of a medium-sized mill on linseed oil without tainting or in any way damaging the cotton oil. It is practicable to run linseed and cotton oil alternately through the same pipes without detriment to either by the exercise of very simple measures. It is done today in England and France on an immense scale. I myself was superintendent of an English linseed-oil mill for four years, and carried out this system, just as I have described. No washing with caustic soda or scraping was ever dreamt of, for the reason that it was unnecessary. Where cotton oil is manufactured for edible purposes it would be an easy matter to reserve the use of certain presses for that purpose exclusively, the remaining presses in the mill being simultaneously used, if need be, on castor, linseed or other oil-bearing seed. The question of pressing flaxseed slowly cuts no figure in the argument, for the reason that if the price suited, it would pay better to press it quickly, exactly like cottonseed. A mill pressing cottonseed for from fifteen to twenty minutes per charge could just as well press a charge of linseed fifteen or twenty minutes longer if it paid to do so. Nothing need be altered to accomplish this excepting the matter of prolonging the processing period, as stated, a circumstance which would be governed by market conditions. For years I have made linseed oil on a 25-minute pressing period here in the United States; this is twenty minutes less than the maximum period mentioned by Mr. Johnson as being necessary, and is a matter of fact exactly the same as that which he considers essential for the extraction of cotton oil.

Eighty per cent. of the linseed oil made today, instead of remaining a protracted period in storage tanks before delivery, as Mr. Johnson states, is shipped before it is a month old. I have seen hundreds of thousands of gallons shipped when only three weeks' old, good, bright oil in every instance. Long storage would improve it, but 20 per cent. of the make is deemed sufficient for this purpose.

Mr. Johnson says linseed oil must be disposed of at retail, "generally in one to five-barrel lots." And a very excellent business, I would supplement, could be transacted on this basis which would probably be a feature of an incipient business in any new location. A five-barrel lot of linseed oil would equal a ten or 12-barrel lot of cotton oil from the standpoint of financial importance, or, in other words, the price of one barrel of linseed oil would be equal to two or two and one-half barrels of cotton oil. But as an instance of the importance of linseed-oil milling, would say, under my personal supervision for the past fifteen years in the United States, shipments of linseed oil were made in lots of from twenty-five to 100 barrels daily, one manufacturing concern I have in mind consuming 100 barrels weekly. The time will come when the manufacturing interests of the

South will demand supplies of this character. Cotton oil would be of no further use for this particular industry to which I refer than water. Will the South make this oil, or permit the North to do so, as now?

Mr. Johnson has reserved his greatest "objection to the last," to quote his exact words, and which is comprised in the fact that cottonseed and flaxseed seasons are contemporaneous. Flaxseed are like cottonseed, will keep without danger of heating or subsequent decomposition for a decade or indefinitely. The cotton-oil manufacturer would not, therefore, be compelled to make a run on linseed unless it suited him; in dry storage it could await his pleasure without detriment to its merits.

Of the 300 or 400 mills in the South, it would be singular if at eight or ten different locations a mill could not be run to advantage, either wholly or in part, for a greater or less period every year. No radical change here; clearly a few mills, selected from their advantageous position for the experiment, would not involve the entire trade.

I am pleased to observe that Mr. Johnson admits his objections are not insuperable. It may be, that after a digest of the facts here hurriedly written, his views in this respect will be still further modified.

In conclusion I would say, I have no special object to serve in making my views public on this question, and merely give the benefit of my knowledge and experience to the Southern cotton-oil men with concurrent suggestions for their acceptance or rejection, as they may deem proper. I might supplement the latter and be pardoned for abstracting a sentence in part from Mr. Johnson's article, as embodied in the following: "One ought to be very familiar with the conditions which exist," not only in the South, but also in the North, before hazarding a criticism on the well-meant and irrefutable statements of one interested in the welfare of the great South.

JOHN BANNON.

Brooklyn, N. Y., September 28.

On Delinting Cottonseed.

By Edwin Lehman Johnson.

[Written for the Manufacturers' Record.]

I.

For two years or more the delinting of cottonseed, or the complete removal by mechanical or chemical means of the short, closely-adhering cotton fibre left upon cottonseed after ginning, has occupied the attention of inventors. Innumerable patents to accomplish this have been taken out, and money reaching up into the millions has been spent in the attempt to make practical application of some or other of these inventions, without any definite result in proportion to the expenditure. Strong claims have been recently made of successful practical and economical machines and methods of accomplishing this object. It is a matter, therefore, not only of great interest, but deserving of careful study and cautious handling.

OBJECT OF DELINTING SEED AND METHODS OF COTTON-OIL MANUFACTURE.

The main object of delinting cottonseed is to permit it to be crushed and pressed entire, so that the hull will form part of the resulting cake and meal, making about double the quantity of cake or meal turned out by the common or decorticated process. There are two distinct methods of manufacturing or "crushing" cottonseed. One is the American or decorticated process, in which the American cottonseed, which have an adhering covering of short lint, are broken by hullers, and the kernels or

"meats" only, after separation from the hulls, are rolled and immediately cooked and pressed. The other is the English or undecorticated process, in which the entire seed, left by nature bare of lint, as in the Egyptian, Brazilian and Sea Island seed, or freed from lint by artificial means, are ground, cooked and pressed with no previous separation of the hull.

The products of the two methods are naturally different. By the American process there are three main products, viz., cottonseed hulls (40 to 50 per cent. of the whole), decorticated cake or meal and crude cotton oil. By the English method there are but two products—undecorticated cake or meal and crude cotton oil.

COMPARISON OF ENGLISH AND AMERICAN METHODS.

It will be seen at a glance that the essential difference between the two is that the English process gets all the hulls into the cake or meal, while the American leaves the hulls a separate and distinct product. A comparison of the products shows that the oil from the American process is lighter in color, sweeter in odor and flavor, less resinous, and makes a much finer refined oil than that made by the English process. The cake and meal made by the American process is lighter colored and much richer in oil, in nitrogen and albuminoids, and more valuable both for food and fertilizer.

In respect to the quality of oil and meal, the American process has a decided advantage over the English. On the other hand, the English process gives about twice as much cake or meal, which, in spite of its inferior quality, and in excess of its real comparative value, finds a ready sale in foreign markets, while the American process leaves its extra product, cottonseed hulls, in a loose, bulky condition, so difficult of transportation and sale as to bring far less than its real value.

If the American manufacturer could get as much for his hulls, as a separate product, as the Englishman realizes for that proportion of his cake and meal which is in reality hulls, the advantage would undeniably rest with the American process, and all the inventors of delinting machines would have their labor only for their pains.

COMPARATIVE COST OF MANUFACTURE.

After the cottonseed are delinted or cleaned, there is practically no difference in the cost per ton of working up the cleaned seed by the English process and the cost of working up the lint-covered seed by the American process, the extra labor and larger crushing plant necessary in the former case about counterbalancing the extra plant, storage and handling necessary in the latter case to hull and separate the seed. As far as dollars and cents go, then, it is virtually only necessary to compare the profits realized from the sale of the oil, meal and hulls of the American process with the oil and meal of the English process to find out what is the margin upon which the inventor or delinter has to work.

MARGIN OF PROFIT IN DELINTING FOR INVENTORS AND INVESTORS.

In other words, in order that the inventor should have a margin to work on, or before it is proved that the delinter is a good thing, it must be shown that the profits upon hulls pressed into cake must be greater than the profits upon hulls sold as a separate product, plus the profits of the finer grade of oil and meal, and that this profit must be enough greater to pay the cost of delinting, to pay for the loss of the lint and a reasonable profit to the delinter. It would be highly interesting, perhaps, to discuss these figures, but it would add little practical value to this

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article and might be misleading, as the prices of seed, cake or meal, oil and hulls, as well as freights, vary so much actually and relatively at different times and places that figures would only be good for a particular locality and time. The writer has no desire to be dogmatic, nor to discourage those who are trying to make money by invention or the application of inventions. He thinks, therefore, a fair statement would be that at certain times and in certain places the delinting of cottonseed for export might be profitable, while at other times, and perhaps in the same places, owing to changed conditions, there might be not only no profit, but an actual loss in delinting. One thing, however, is certain, that if cottonseed hulls, as a separate product, ever get to sell for their intrinsic value, delinting patents will not be worth the paper upon which they are written.

This first paper is confined to the treatment known as delinting cottonseed for export.

CAUTION TO INVESTORS.

Enough, perhaps, has been said to show that this is not so simple a question as first appears, and that those who are looking at this matter with a view to investing either their brains or their capital should get all the figures I have indicated are necessary, applicable to the particular place where it is proposed to delint the seed, and make a careful study of the markets and conditions of the cotton-oil industry, both in this country and abroad.

I have written not for the purpose of throwing any obstacle in the way of any legitimate enterprise, but to caution inventors and investors that there is no bonanza in delinting cottonseed, and the same prudence, caution and careful investigation are necessary in this enterprise as in all new enterprises.

The second paper will discuss delinting of seed for manufacture in the United States.

Memphis, September 16, 1896.

The Market for Cottonseed Products.

New York, N. Y., September 29.

Cotton oil is in a stronger position today than at any previous time in many months. The upward tendency of animal products, which has long been held in check, and actually depressed by financial uncertainties, has stimulated the demand for cotton oil, while concurrently strengthening values. Wheat has already derived material benefit from the changed order of affairs, while animal products are now slowly but surely following suit. Notwithstanding the enormous supply of corn, which we have been all along reminded of, the material is advancing in price. The demand for oil for present and future delivery has been exceptionally good, although a scarcity of freight room for export is still a current feature. Only a week or two ago we reported cotton oil relatively higher at this market than at the producing centres, but conditions have changed, and in some instances are actually reversed. Soapmakers, lard-compounders and shippers are enquiring concerning futures, and the market generally assumes a bullish air. Chicago lard is quoted at $4\frac{1}{2}$ cents, and 4 to $4\frac{1}{8}$ cents for Western compound lard. Tallow has also advanced $\frac{1}{4}$ c. It will be noticed that every competing product on the list has advanced, thus practically assuring an upward movement in cotton oil. Several important transactions have just been closed in prime yellow on a 24-cent basis. The demand for new crude for the West, direct from the mills, has been active. Sales of crude are reported at the Atlantic coast of prime at 16 to $16\frac{1}{2}$ cents f. o. b. mills, while sales aggregating 1840 barrels of crude are reported from Alabama at 16 cents f. o. b.

At Memphis $17\frac{1}{4}$ cents is asked for prime crude f. o. b. at the mills, and in Texas, interior, $14\frac{1}{2}$ to 15 cents is quoted. Arrivals aggregated 3000 barrels, while upward of 7000 barrels were exported, four-fifths of which was destined for Marseilles and Trieste. The closing prices for cotton oil are as follows: Crude, prime, 21 to 22 cents; loose, f. o. b. mills, $14\frac{1}{4}$ to 17 cents; off-grade crude, 19 to 20 cents; prime summer yellow, $23\frac{1}{2}$ to 24 cents; off-grade summer yellow, 23 to $23\frac{1}{2}$ cents; prime winter yellow, 30 to 32 cents; yellow, butter grades, 25 to $26\frac{1}{2}$ cents; prime white, $25\frac{1}{2}$ to 26 cents, and $\frac{5}{8}$ to $\frac{3}{4}$ c. for soap stock. Cotton oil in London is advancing.

Cake and Meal.—The market is firm, and abroad prices are hardening. Prime cake is quoted at \$11.50 to \$13 per ton, Houston. Cotton meal is quoted here at \$19 to \$20 per ton; no cake on offer. Exports are improving in volume, heavy shipments of cake and meal being reported from Galveston and New Orleans to Liverpool and Hamburg.

Cottonseed-Oil Notes.

The Interstate Cotton Oil Co., of Augusta, Ga., commenced to operate its plant last week. The company is well equipped for a good season's crushing. Seed in Augusta sells at \$9 to \$10 per ton.

The Ennis Cotton Oil Mill, at Ennis, Texas, was destroyed by fire on the 22d ult., supposed to be the work of an incendiary. The mill was owned and operated by John & W. Guitar. The loss is said to be almost a total one, and is covered by about \$27,500 insurance.

Mr. N. L. Scales, secretary and general manager of the Corsicana Cotton Oil Co., of Corsicana, Texas, in a letter to the Manufacturers' Record states that the two oil mills at that place have been running regularly since September 1. Receipts of seed are ample for all requirements, planters in that section selling seed freely.

The seedhouse of the Giovanovitch Oil Co., at Natchitoches, La., was burned on the 26th ult. At the time of the fire there was about \$1500 worth of cottonseed stored in the building, which is a total loss. The total loss is estimated at \$5000, with \$3000 insurance. The building will be rebuilt at once in order to be ready for the crushing season.

Among the foreign exports of cottonseed products from Galveston, Texas, last week the following are reported: For Rotterdam per steamship Norze 4878 sacks of cottonseed meal and 225 barrels of cottonseed oil; for Bremen per steamship Lambert's Point 3730 sacks of cottonseed meal, and for Hamburg by the same vessel 15,625 sacks of cottonseed meal, 3164 sacks cottonseed cake and fifty barrels of cottonseed oil.

The Texas cottonseed-oil mills report a big demand for cottonseed meal and cake, and the market is firm in tone, with an advancing tendency. Manufacturers hold their stocks firmly, and many refuse to sell except at figures above present quotations. Receipts of cottonseed are not up to the full requirements of the mills, and a number of manufacturers have not commenced to crush seed. Oil does not show the strength of other cottonseed products.

The market for cottonseed products at New Orleans is reported very quiet; prime crude oil in bulk, 16 to $16\frac{1}{2}$ cents; prime summer yellow in barrels, $22\frac{1}{2}$ cents; meal, \$16.25 to \$16.50 per short ton, and cake, \$17 to \$17.50 per long ton for export. Receivers' prices are quoted as follows: Cottonseed, \$7 per ton (2000 pounds) delivered net to the mills; cotton-

seed meal jobbing at depot, \$16 to \$16.25 per short ton, and \$18 per long ton for export f. o. b.; cottonseed oil, $18\frac{1}{2}$ cents per gallon for strictly prime crude; in bulk, 16 cents, and 23 to 24 cents for refined oil at wholesale or for shipment; oilcake, \$18 per long ton f. o. b.; foots, $\frac{1}{2}$ c. to $\frac{3}{4}$ c.; linters—A, $3\frac{3}{4}$ to $3\frac{1}{2}$ cents per pound; B, $3\frac{1}{2}$ to $3\frac{3}{4}$ cents; C, 3 to $3\frac{1}{4}$ cents; hulls delivered at 20 to 25 cents per 100 pounds, according to location of mills.

A conference of cottonseed-oil-mill men was held in Columbia, S. C., last week. The meeting was a strictly representative one, and harmonious in every feature. Among the main discussions were the conditions that at present confront the cotton-oil industry, and to devise a method to dispose of the by-products and regulate the sale of oil. Among the millmen present were the following: H. C. Wells, Wilmington, N. C.; H. L. Odionine, Florence, S. C.; R. J. Blackwell, Marion, S. C.; J. H. David, Dillon, S. C.; C. S. McCullough, Darlington, S. C.; P. Moses, Sumter, S. C.; A. C. McFall, Bennettsville, S. C.; W. J. Montgomery, Atlanta, Ga.; Fred. Oliver, T. I. Davis, J. F. Anderson, D. B. Kimball, Charlotte, N. C.; T. O. Wilbur, Charleston, S. C.; J. T. Thornhill, Charleston Refinery; H. C. Mikel, A. J. Little, Elberton, Ga.; W. E. Thompson, Union, S. C.; Alex. De Long, Spartanburg, S. C.; J. Allen Smith, Abbeville, S. C.; B. C. Johnson, Easley, S. C.; A. H. Careton, Greenville, S. C.; Garland Jones, Raleigh, N. C.; B. H. Hudgins, Laurens, S. C.; C. Fitzsimmons and M. C. Robertson, Columbia, S. C.

T. G. Bush, president of the Clifton Iron Co., Alabama, in a letter to the Manufacturers' Record, says: "I notice in one of our State papers that the Clifton Iron Co. is reported as non-committal to your journal with reference to the affect on business of the gold or silver standard. I beg to say, in behalf of the Clifton Iron Co., that it is decidedly of the belief that the maintenance of the gold standard will be the surest means of reviving business of this and other sections, which is now so completely prostrated. It is my belief that with a decided majority in the November election for sound money, that confidence will be restored and business will gradually assume its normal condition, with a prospect of continued improvement. It is my belief that the success of the silver ticket would continue our state of uncertainty and make the date of resumption of business indefinite, inasmuch as everything would have to be adjusted to a new standard. The process of this adjustment will doubtless cause great loss and disaster. I believe in a reasonable protection to the industries of this country, but not extreme measures, as have been advocated by many of the republican party in the past. The necessity for protection that once prevailed does not now exist."

Doryles Putnam, president Ashland Coal & Iron Co., Ashland, Ky.: "Should the gold and protection ticket win in November, I believe there will be a gradual increase of confidence and an improvement in business. Should the silver and free-trade ticket win in November, I fear its result would be such a shock to the present timid financial condition as to cause great distress and embarrassment, from which those dependent on their labor for support would be the first to suffer."

Carnegie Iron Co., Johnson City, Tenn.: "In answer to your question we have to say: 1. The revival of business at once. 2. Utter disaster, at least for some time to come."

TEXTILES.

[A complete record of new textile enterprises in the South will be found in the Construction Department.]

Correspondence relating to textile matters, especially to the cotton-mill interests of the South, and items of news about new mills or enlargements, special contracts for goods, market conditions, etc., are invited by the Manufacturers' Record. We shall be glad to have such matter at all times, and also to have any general discussion relating to cotton matters.

Wants to Buy Yarns.

A Boston commission house wishes to arrange with one or more mills making dressed and undressed yarns on beams, numbers ranging from 8s to 30s. The yarn is wanted for the Chicago and St. Louis markets. The mill must be near enough to the above centres so that the freights will not be prohibitory to our selling at the market price. Address "Cotton," care S. I. Carpenter, John Hancock Building, Boston.

Textile Notes.

The Swift Creek Cotton Mills, near Petersburg, Va., are approaching completion, and the machinery is now being placed in position. The plant will start with 5000 spindles and 150 looms, making 40-inch goods. A new water-wheel and a 100 horse-power engine will also be put in.

The Piedmont Manufacturing Co., of Piedmont, S. C., is making extensive improvements to its plant. Orders for additional revolving flat cards, electric stop-motion drawing-frames, slubbing and roving frames, etc., have been ordered from the Howard & Bullough Machine Co., of Pawtucket, R. I.

Superficial Examination of Ores Free

The Manufacturers' Record, seeking to turn attention to the mineral resources of the South and to aid its readers in the intelligent investigation as to what may or may not be of value, has arranged with Mr. Charles Catlett, chemist and geologist, of Staunton, Va., to make superficial examinations of samples without charge. Such samples will be reported in the Manufacturers' Record, with a word of explanation as to probable or possible value. Such an examination is necessarily superficial, and does not comprise a quantitative analysis, which, where desired, is charged for.

The following directions should be observed:

Samples should be sent to Charles Catlett, No. 10 New Court House street, Staunton, Va., with all mail or express charges prepaid. The name and address of sender should be clearly stated on package.

Care should be taken to select an average sample, otherwise the examination will prove of no value. Quite a small quantity will be sufficient.

A postal card or letter to Mr. Catlett, with a word of explanation as to mode and exact location of occurrence, would assist in giving value to the examination.

O. B. Havis, La Follette, Tenn.: Sulphide of antimony carrying lead and possibly other minerals; valuable in quantity.

W. R. Hautz, Roanoke, Va.: No. 1. Slate containing pyrites; No. 2. Calcite (carbonate lime); No. 3. Decomposed felspathic rock; No. 4. Micaceous schist; No. 5. Clay; No. 6. Felspathic rock containing hornblende; No. 7. Red clay. None of these samples give promise of value.

R. Mays Cleveland, Marietta, S. C.: No. 1. Contains no nickel; color due to manganese; No. 2. Sandstone; no indication of coal.

J. A. Kline, London, Tenn.: No. 1. Sulphide antimony carrying lead and arsenic; No. 2. Mineral paint.

If you wish to keep posted on the progress of the South, read the Manufacturers' Record. Price \$4.00 a year.

MECHANICAL.**Charlotte's Fine Courthouse.**

A feature of improvements in the South is the many new municipal buildings that have been erected, are under contract or in contemplation. Consider-

able attention is given to the introduction of progressive ideas in these structures, and as a result there is to be seen many pleasing examples of architecture in this direction. Herewith is illustrated the new Temple of Justice for Mecklenburg county, North Carolina, now under construction. It will be one of the best

floors and yard, wood finish, add to the beauty of the building. It is regarded as a remarkable building for the cost, \$50,000, and much credit is given the county commissioners for the wisdom of their choice. The architect, Frank P. Milburn, of Winston, N. C., was selected, after plans prepared by twenty-seven

Lightning Feed Rip-Saw Table.

One of the latest improved, heaviest and most powerful self-feed saw tables now built is illustrated herewith. It is designed for all kinds of ripping of any width up to eighteen inches wide and eight inches thick. The iron frame is

opening around the saw, into which an iron plate is accurately fitted, which is taken out when saws are to be changed, thus avoiding lifting up the table. The iron table is raised or lowered vertically by an improved patent table-rising device, with a crank at the front end, as shown in the cut. It is provided with two long idle rolls, to relieve it from friction and wear. The feed works are powerful and reliable, capable of doing rapid work, and are started and stopped by a tightener. Feed-roll marks are taken out of the saw, leaving no mark on the lumber.

The out-feeding roll of the saw is an important feature to the power feed, as it carries all the stock out clear from the saw; also prevents the lumber from twisting and coming in contact with the back of the saw. It also has three rates of feed—seventy-two, 132 and 190 lineal feet per minute, respectively. The method of changing from self-feed to hand-feed by means of a hand-wheel, shown at front of machine, is simple, rapid and easy; and the feed arm is self-adjusting for irregularities in thickness of lumber.

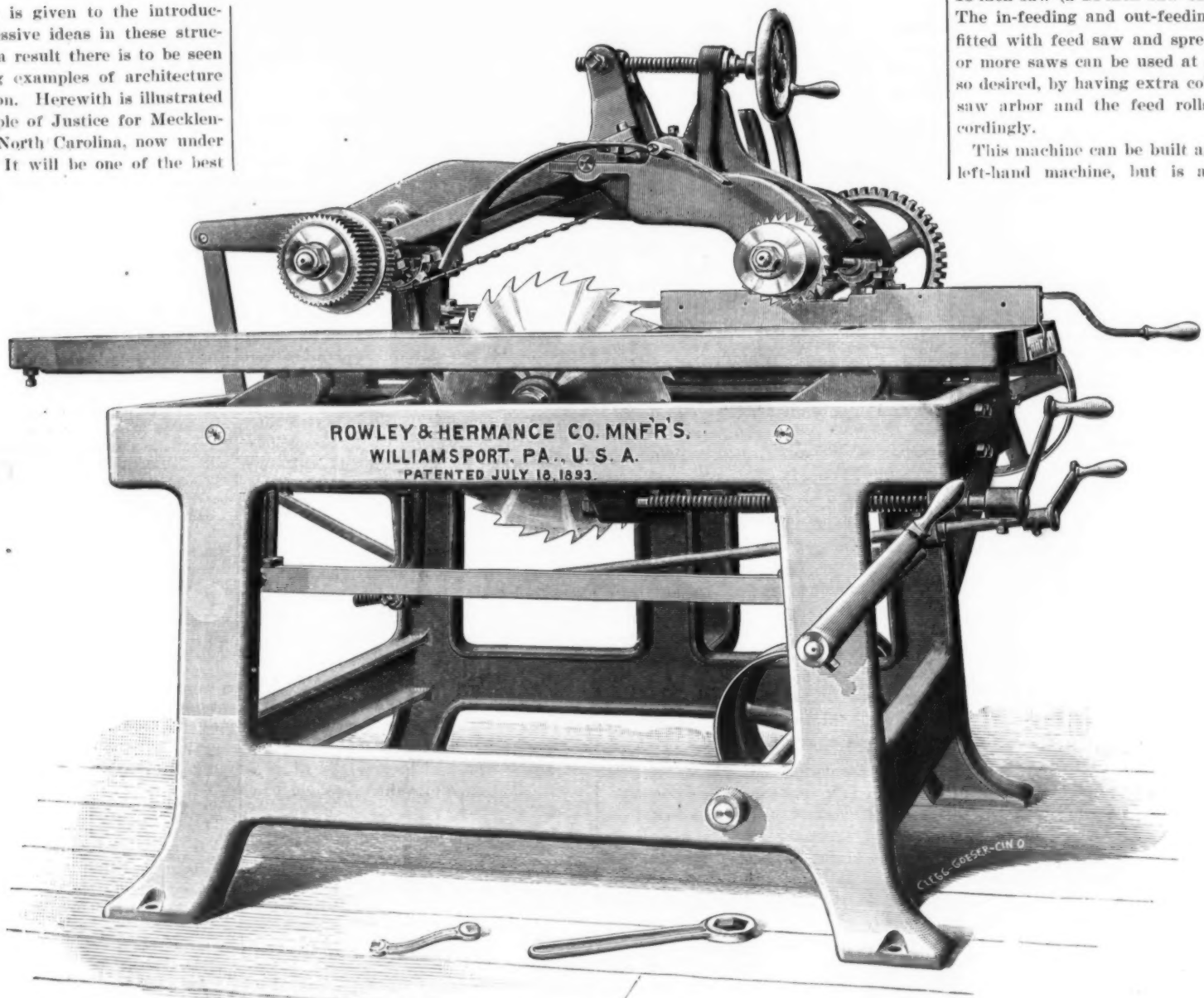
A patent setting device for adjusting the gage is entirely new and novel. The gage can be set at any mark on the index plate, or at any fraction of an inch, and by a slight movement of the lever is locked rigidly in position.

With each machine is furnished one 18-inch saw (a 24-inch saw can be used). The in-feeding and out-feeding rolls are fitted with feed saw and spreader. Two or more saws can be used at one time if so desired, by having extra collars on the saw arbor and the feed rolls fitted accordingly.

This machine can be built as a right or left-hand machine, but is always fur-



NEW COURTHOUSE, CHARLOTTE, N. C.



LIGHTNING FEED RIP-SAW TABLE.

in the State. It occupies the site of Queen Anne's Museum, built in 1776 on Tryon street. The building is of fire-proof construction, 75x120 feet, with granite foundations and approaches. Buff brick and buff stone facade, marble

prominent architects from different parts of the United States had been examined. The principal entrance is quite imposing, with open portico above surmounted by a large dome of pleasing proportions.

cast in one piece, very heavy and strong, with a substantial outside bearing for the saw arbor, strongly braced to the side and the bottom of the frame. The table is iron, three feet eight inches wide by five feet two inches long, with an

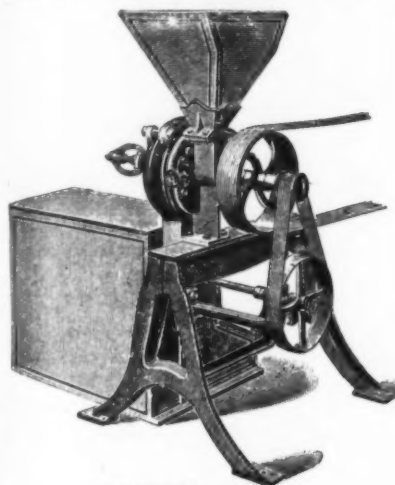
nished as a right-hand machine unless otherwise ordered. Weight 2000 pounds. Size of pulley on arbor 8x8, and should run 2000 revolutions per minute.

Countershaft furnished at extra cost when ordered, with tight and loose pul-

leys 12x8, and should run 700 revolutions per minute. It is manufactured by the Rowley & Hermance Co., Williamsport, Pa., U. S. A.

Duplex Grinding Mill.

An all-around feed mill which is finding favor by its good work is illustrated herewith. The cuts show interior and exterior views of this machine. It is known as the Duplex grinding mill, and is especially adapted for grinding ear corn alone or with shucks on. It is provided with new double breakers and all

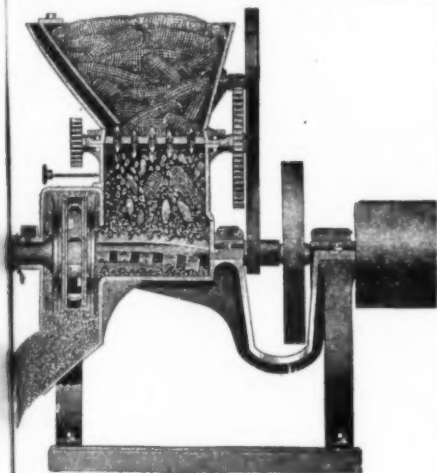


EXTERIOR VIEW.

late improvements, and is constructed in a substantial manner.

The interior view shows the action of the mill. The frame, fly-wheel, pulley, main shaft, hopper, double breakers and gearing, belt and pulley for operating same, the lower crusher, the duplex grinding plates or burrs and case enclosing same are all correctly shown.

This corn, cob and shuck mill is in general use throughout the country on



INTERIOR VIEW.

account of doing a variety of work. The improved results obtained, reduction in power and production of a better grade of feed are points in its favor.

A complete extra set of four rings, enabling the buyer to grind upwards of 5000 bushels, is furnished with every mill. The Duplex grinding mill is manufactured in four sizes by the Paragon Mill and Machine Works, 44 South Jefferson street, Chicago, Ill.

New Boring, Drilling and Milling Machine.

Advances made in the construction of machine-shop tools are of special interest to a large circle of manufacturers and mechanics. The accompanying illustrations show a new combined horizontal boring, drilling and milling machine. The front part of the machine is like an ordinary commercial horizontal boring and drilling machine, and the rear of the machine is of a new and improved design. The boring bar of the machine is four and one-quarter inches in diameter, and has a continuous feed of forty-two

inches. The sleeve which surrounds the boring bar is driven with a triple lead phosphor-bronze spiral gear and Harvey-ized steel pinion; the driving cone is set

circular carriage, special fixtures and jigs, attach special interest to its advantages. This machine is described as more powerful and with a greater range

adapted to any work within its range.

This machine is being introduced by the Newton Machine Tool Works, Vine and 24th streets, Philadelphia, Pa.

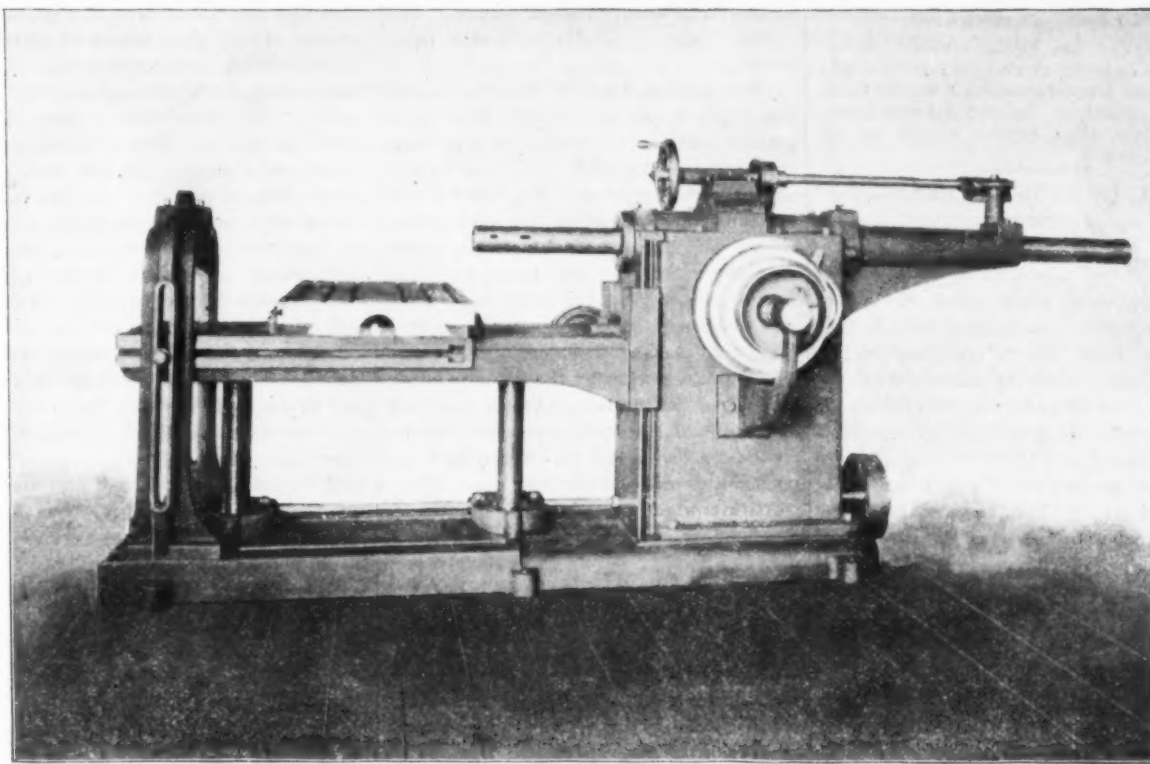


FIG. 1.

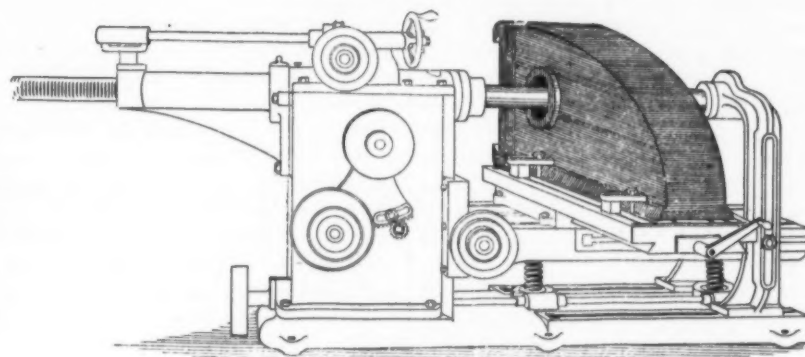


FIG. 2.

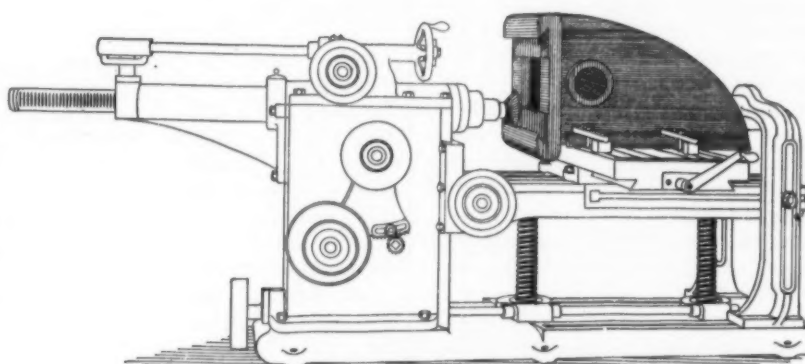


FIG. 3.

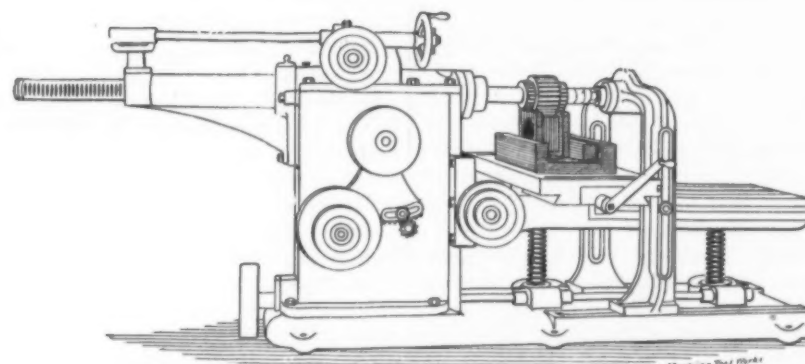


FIG. 4.

at right angles to the axis of the boring bar. On the cone shaft are the back gears, and with this driving mechanism the machine, it is claimed, has from 50 to 100 per cent. more power than an ordinary one of the same capacity. The feeds have a very wide range, adapted for fine feeds for drills, to the coarsest feed for finishing cuts for boring, and all the different ranges required, from the small milling cutters to the large facing milling cutters. On the end of the driving shaft is a cone pulley, which is belted to the cone pulley of the quadrant, and, by belting to the cone pulley above the boring bar, the boring feed is put in operation. With the same belt, adjusted to the rear cone pulley on knee, the milling feed can be used, operating the carriage as a milling machine, with automatic stop, the same as the ordinary milling machine. The knee, or worktable, is made either six or nine feet long, and has a power adjustment through a separate countershaft. This machine is specially adapted to locomotive repair shops, and machine shops in general can use it with economy. It is very seldom that there is a piece of work to be machined on a boring machine that does not have other parts of it that could be milled to advantage without unclamping the piece, that is, if the machine is adapted to it. With the ordinary horizontal boring and drilling machine, the boring and drilling are done and the work transferred to a planer or milling machine for the other operations. With this machine, in a great many cases, it is not necessary to transfer the work, as is shown in the cuts. Fig. 2 shows one of these patent duplex milling machine uprights, the hole for the overhanging arm being bored. Fig. 3 shows the upright having the different dovetail ways milled. Fig. 4 shows the machine being used as a heavy milling machine, milling a set of rod brasses. This first operation of boring and milling the duplex upright could be finished with one clamping, if the circular carriage be used on the machine.

Circular carriage will be furnished either with or without automatic feed, and of any desired diameter, at an additional price.

The possibilities of this machine, with

than the ordinary machine, and, besides, is capable of doing heavy milling work to good advantage. The machine admits work to the diameter of sixty-five inches, is a heavy, substantial tool, and

If you are thinking of enlarging your mill, factory or mine, or of purchasing machinery of any kind, send us a postal card giving the character of the machinery needed.

LUMBER.

[A complete record of new mills and building operations in the South will be found in the Construction Department.]

Lumber Directory.

Readers of the Manufacturers' Record who may be in the market for lumber of any description are recommended to the directory of Southern lumber manufacturers and dealers which appears among the advertising pages.

FACTS ABOUT CYPRESS.

Government Investigations and Experiences of Lumbermen.

The increasing consumption of cypress makes timely the following article, prepared by E. J. Marsh, secretary of the Atlantic Coast Cypress Association:

During the introduction of cypress as a high-class wood, and as an equal of white pine, the buyers and consumers have been puzzled over the fact that manufacturers, by wide advertisements, verbal statements and positive assertions, have created the impression that a very great difference in the quality and color exists.

Over two years ago the Agricultural Department at Washington, owing to the numerous inquiries made to it on this subject, considered the matter of such importance that it authorized the Department of Forestry to make a minute and exhaustive research on the subject of cypress generally; to ascertain, among other things, as to its quality and color, and to determine beyond a doubt the truth relative to the wood grown in different sections of the country, it being claimed by certain manufacturers that the cypress grown in their particular localities was superior to any other in quality and color. For this reason alone it was highly desirable to give truthful information to the public upon this matter.

While the manuscript is not yet compiled for publication, the Department has generously given me the results of Dr. Roth's (the timber physics expert) conclusions in advance. The manuscript is very exhaustive on all characteristics of the wood. But for the purposes of this brief article only the question of quality and color will be quoted. The Division of Forestry's report states:

I. No essential superiority exists in the cypress of one section over that of another.

II. The color of cypress depends upon the age of the tree.

III. The older the tree, the darker the color.

IV. Fineness of grain and quality depends upon the density of the wood; the heavier the weight, the finer the grain.

V. Mature trees range from 300 to 400 years old.

VI. As the result of experiments on the question of fine grain, taken from trees in the localities named, specific weight is shown as follows: Georgia, 38; Louisiana, 38; Mississippi, 51; South Carolina, 58.

VII. Color and quality is affected by conditions, and is divided into: "Pine Barren," or "Pond," black in color; "River Bottom," light; "Coast," reddish; "Upland," white.

VIII. Heavy and light wood is often found side by side, and seems to depend chiefly upon the rate of growth; the faster the tree has grown, the lighter the wood.

IX. Weight for weight. Cypress shrinks about as much as pine.

X. The strength of cypress is: Green, 4500 pounds compression, 6500 pounds bending; seasoned, 6750 pounds compression, 9750 pounds bending.

XI. The age of the tree is, in any for-

est, variable. Young and old trees occur in all localities.

XII. The rate of growth of cypress is extremely variable, a tree sometimes requiring 300 years to attain the diameter of fifteen to twenty inches.

The report concludes with this paragraph:

"Whether one kind or the other is better depends on their use. The light-colored is easier worked, shrinks less, serves much like white pine, but having a great advantage over this wood in its durability. The darker, fine and sharp-ringed varieties are much prettier, better suited for panel work wherever their handsome, unobtrusive grain is valued, and, moreover, they are stronger in proportion as they are heavier."

The investigation by the government is the only scientific research ever made on cypress, and its conclusions must be accepted by the public as correct, and the proper authority on the subject.

All manufacturers, in strife for trade, have endeavored by every means to prejudice the opinion of buyers in their favor. But it seems to me that it is better for the future of cypress to have the public know the truth, that there is no difference arising from geographical lines. Each section has its old trees and its young trees, its dark colors and its light colors, its heavy weight and its light-weight wood, and, as Dr. Roth says, either variety may be grown in company with the other.

Some of the information contained in the government's report has been known to manufacturers, but has been of no use to the general public, as when such knowledge has been given by advertisement, or otherwise, the facts have been so twisted or colored as to point to the conclusion that the particular locality in which the narrator operates is the only true home of cypress of the best quality.

Statements of several of the most prominent persons engaged in the manufacture and introduction of cypress can be quoted from the minutes of the meeting held in New Orleans, February 25, by a committee of the Atlantic Coast Cypress Association and a committee of the Southern Cypress Lumber and Shingle Association. During the discussion of ways and means to bring the two associations into harmonious co-operation, President G. M. Bowie, of the Louisiana Association, made the following statement: "I am familiar with the Atlantic coast cypress, and have never considered that there was any material difference between it and the Louisiana stock; the difference is only what has been created in the minds of the public by the wise business methods of this (his) association."

Director Jos. Rathborne, of the Louisiana Association, stated that he was manufacturing both in Louisiana and on the Atlantic coast of South Carolina, and knew there was but little, if any, difference between the Louisiana and Atlantic coast stock.

President A. E. Wilson, of the Atlantic Coast Cypress Association, stated that he had never been able to discover any difference in the cypress grown in swamps subject to frequent overflow, wherever located.

James D. Lacey, of New Orleans, one of the largest, if not the largest, holder and dealer in cypress lands, stated in the above meeting that he held large tracts of cypress lands both in Louisiana and along the Atlantic coast, and from his opportunities for judging, he had come to believe firmly that there was no difference in the cypress growing in either section, under corresponding conditions. He thought the combined action of the Louisiana Association, through wide adver-

tising in the press, had created the impression in the minds of the uninformed that there was some difference.

By referring to the files of a certain lumber journal for 1895, it will be found that this subject of difference in cypress grown in different localities attracted their attention and investigation. In the issue referred to, their representative reports having interviewed certain men engaged in logging cypress in swamps on the Atlantic coast, who had been raised on the Mississippi river, and had engaged continually for years in logging cypress in Louisiana. The testimony of these men, without exception, was to the effect that as a whole, there was no discernible difference between the cypress growing in the two localities. The journal extended its investigation of the subject to sawyers and planing-mill foremen (as recited in detail in the said report), and from none could any opinion be elicited that they had noticed any difference, in their respective occupations, of sawing the log, or working it into various forms in the planing mill.

From my own observation of six years logging and manufacturing in Louisiana (a member of the Louisiana Association), and three years logging on the coast (a member of the Atlantic Coast Association), and having probably visited the mills of every operator in the country, I have had full opportunity for arriving at an opinion on the subject, which is, that where the same conditions of growth exist, there is absolutely no difference in the quality or color of the cypress.

On visiting the yards of consumers, it always is noticeable that a car of black cypress might come from the Atlantic coast and a car of light cypress from Louisiana, and the reverse may be true. This is fully explained in the foregoing government report.

A careful consideration of these facts, to my mind, should prove conclusive, and set at rest the feeling existing among new users of cypress, that great caution should be used, in placing orders, as to the locality from which it is to come.

LUMBER MARKET REVIEWS.

Baltimore.

Office Manufacturers' Record,
Baltimore, Md., October 1.

There is very little change to note in the local lumber market, and the volume of business has not expanded to any great extent. There is a moderate demand from boxmakers and planing mills for such lumber as they require, but only in small lots to satisfy present wants. Receipts of air-dried yellow pine are fully ample for the demand, and stocks are generally well assorted. There is some business reported in kiln-dried North Carolina pine, and there is a better inquiry from out-of-town buyers. White pine is moving more freely, and prices are steady at quotations. Cypress is easy, with very little demand. The hardwood business shows no improvement, and the demand locally, as well as from out-of-town parties, is light. The export trade is fair, but the volume of business reported not what it should be, as shippers' prices do not meet the views of British buyers. The sudden and sharp rise in freights, owing to the scarcity of tonnage, has a depressing effect here on the export of hardwoods.

The following list represents the prices current at this date:

[The quotations for yellow pine are for cargo lots, and for all hardwoods the figures indicate values for choice car lots.]

VIRGINIA AND NORTH CAROLINA PINE
5-4x10 No. 2, kiln dried.....\$12 50@ 13 50
5-4x12 No. 2, kiln dried.....17 50@ 18 50
4-4x10 No. 1, kiln dried.....15 00@ 15 50
4-4x12 No. 1, kiln dried.....15 00@ 16 00
4-4 nar. edge, No. 1, kiln dried. 13 00@ 14 00
4-4 wide edge, No. 1, kiln dr'd. 17 00@ 18 00
6-4x10 & 12, No. 1, kiln dried. 23 00@ 24 00

4-4 No. 1 edge floor., air dried. 13 50@ 14 50
4-4 No. 2 edge floor., air dried. 10 50@ 11 50
4-4 No. 1 12-in. stock, air dried. 13 50@ 14 50
4-4 No. 2 12-in. stock.....11 00@ 12 00
4-4 edge box or rough wide....7 50@ 8 50
4-4 edge box do. (ord. widths). 6 50@ 7 00
4-4 12-in. rough.....9 00@ 9 50
3/4 narrow edge.....5 00@ 6 00
3/4 wide.....6 50@ 7 50
3/4x9 1/2 and 10 1/2-inch.....8 00@ 9 00
Small joists, 2 1/2-12, 14 and 16 long.....7 00@ 8 00
Large joists, 3-16 long and up 8 00@ 9 00
Scantling, 2x3, 2x4 and 3x4...7 00@ 8 00

WHITE PINE.

1st and 2d clear, 4-4, 5-4, 6-4 and 8-4.....47 50@ 48 50
3d clear, 4-4, 5-4, 6-4 and 8-4...42 50@ 43 50
Good edge culls.....14 50@ 15 50
Good stock.....16 50@ 17 50

CYPRESS.

4-4x6, No. 1.....17 50@ 18 50
4-4x6, No. 2.....12 50@ 14 50
4-4x6, 16 feet, fencing.....10 50@ 11 50
4-4x6, rough.....8 50@ 9 50
4-4 rough edge.....8 00@ 8 50
4-4 edge, No. 1.....16 00@ 17 00
4-4 edge, No. 2.....11 00@ 12 00
Gulf, 4-4, Nos. 1 and 2.....28 50@ 30 50
Gulf, 6-4, Nos. 1 and 2.....31 50@ 32 50

HARDWOODS-WALNUT.

5-8, Nos. 1 and 2.....65 00@ 75 00
4-4, Nos. 1 and 2.....80 00@ 90 00
5-4, 6-4 and 8-4.....85 00@ 95 00
Newell stuff, clear of heart...85 00@100 00
Culls.....20 00@ 30 00

OAK.

Cabinet, white and red, Southern, plain-sawed and good, 1 and 2, 8 inches and up, 12 to 16 feet long, 4-4.....30 00@ 34 00
Quartered white, Western, 1 and 2 quality, all figured, 6 inches and up wide, 4-4.....53 00@ 55 00
Culls.....10 00@ 15 00

POPULAR.

Nos. 1 and 2, 5-8.....24 50@ 25 50
Nos. 1 and 2, 4-4.....28 00@ 30 00
Nos. 1 and 2, 6 and 8-4.....32 50@ 33 50
Culls.....13 00@ 16 00

SHINGLES.

Cypr., No. 1 h'rta, sawed, 6x20. 7 00@ 7 50
No. 1 saps, sawed, 6x20.....5 00@ 6 00
No. 1 hearts, shaved, 6x20....6 00@ 7 00
No. 1 saps, shaved, 6x20.....5 00@ —

LATHS.

White pine.....2 65@ 2 70
Spruce.....2 10@ 2 20
Cypress.....2 10@ 2 20

Charleston.

[From our own Correspondent.]

Charleston, S. C., September 28.

The volume of business in the local lumber market during the week under review has been moderate, while the demand is steady and orders not large. At Georgetown there is quite an active movement among lumbermen. The Clyde Line wharves there are generally crowded with freight, and it is stated that this company is now doing the heaviest business of the year. The Gardner & Lacy Lumber Co. and Mr. Jacob Savage are turning out large quantities of lumber, and their shipments both by water and rail are increasing. The Gardner & Lacy Lumber Co. has contracted for a private system of electric lights in its mill, which will greatly facilitate business. The demand for cross-ties is good, and the shingle business is also reported fairly active. The market for desirable grades of lumber is steady and prices still very low, while manufacturers have to exercise the greatest economy to get anything out of the business at present prices. Quotations ranged on Saturday as follows: Merchantable lumber, \$14 to \$16 for city-sawed, \$12 to \$14 for railroad; square and sound, \$9 to \$13 for railroad, \$8 to \$11 for raft; dock timber, \$4.50 to \$6.50; shipping, \$8.50 to \$10.50; shingles, \$5 to \$7 per thousand. Among the shipments during the past week the following vessels are reported: Steamship Calla for Jamaica with 35,000 feet of lumber and 150,000 shingles, and schooner Tillie Vanderherchen for New York with 313,000 feet of lumber. The schooner Edna & Emma was loading at Georgetown. The total shipments of lumber from this port since September 1 amount to 2,274,759 feet to domestic ports and 85,000 to foreign, making a total of 2,359,759 feet, against 6,595,653 feet for the corresponding period last year. Freights are steady, with a scarcity of desirable tonnage, and rates to New York are quoted \$4.38 to \$4.50; ties, 14 to 14 1/2 cents; oak ties to Philadelphia, 19 cents. The charters reported in New York last week were as follows: A schooner, 547 tons, from Charleston to New Haven at \$4.50; a schooner, 333

tons, from Georgetown, S. C., to Barbadoes with lumber at \$5.50, and a schooner, 381 tons, from Charleston to New York at \$4.45.

Savannah.

[From our own Correspondent.]

Savannah, Ga., September 28.

There have been a number of complaints among millmen in this section that the lumber market is exceedingly dull, while, on the other hand, there are those who take an opposite view and say they have all the orders they want. There is, however, the usual shipments going out every week, but not of such volume as for the corresponding period last year. During the past week sales have been light, and shipments also have fallen off some. At milling points in the interior the output is being curtailed, and stocks are not allowed to accumulate. At Cordele a prominent millman, in discussing the lumber situation in that section, said: "It's all nonsense to talk about dullness in the lumber market. I don't think there will be any scarcity of orders from now on. Orders for 5,000,000 feet of lumber will be placed near here in the next sixty days." The lumber business at Darien is reported fairly active, and in timber the exports are fair. As to prices, they show no improvement, although certain grades are a little firmer. The market closed on Saturday as follows: Ordinary sizes, \$11 to \$12; difficult sizes, \$13 to \$18; flooring boards, \$15 to \$22; shipstuffs, \$16.50 to \$20, and sawn ties, \$10. There has been more inquiry for crossties during the past week, and several large lots have been contracted for. Among the shipments of lumber and other wood products during the week, the following vessels are reported: Schooner Island City for Baltimore with 289,222 feet of pitch-pine lumber, and the steamships Berkshire, 65,890 feet, and the D. H. Miller, 118,940 feet, for the same port; steamship Gate City for Boston with 150,665 feet of pitch-pine lumber; steamship City of Macon for Philadelphia with 58,372 feet, and New York steamers with 360,000 feet. The Russian bark Linda Morgenroden cleared for Hamburg with 6569 white-oak staves among her cargo. The Georgia Pine Lumber Co. shipped on the bark Martha C. Craig, for Pernambuco, 72,403 feet of pitch-pine lumber. Among the charters reported in New York last week were the following: Schooner Blanch Hopkins, 602 tons, from Brunswick to Kingston, Ja., with lumber at \$5.50; a schooner from Fernandina to New York with lumber at \$4.50; schooner A. D. Lamson, 426 tons, from Savannah to Philadelphia with lumber on private terms; a schooner, 866 tons, from Brunswick to St. Jago with ties at \$5.75, American gold, and a schooner, 436 tons, from Savannah to Philadelphia at \$4.25.

Jacksonville.

[From our own Correspondent.]

Jacksonville, Fla., September 28.

The lumber market here has shown a fair amount of activity during the present month, but the volume of business will not likely be as large as that of August. Of course, the financial and political agitation has considerable to do in the work of creating the present depression in the market. There is, notwithstanding, a good demand for the best grades, and prices are very steady, holders not wishing to sell below list prices. The shingle and crosstie industry is becoming of more importance every year, and at the moment there is a good demand for all that can be produced. Saw mills in this section are generally supplied with orders, and stocks are not accumulating to any great extent. The demand from the West Indies and other foreign ports is better, and shipments so far this year are largely

in excess of last. During the past week the following vessels cleared and sailed from this port: Schooner Sadie Wilcutt for Demerara with 215,000 feet of lumber; schooners Eva A. Danenhower with 581,000 shingles and 71,000 feet of lumber, and E. A. Scribner with 260,000 feet, both for Philadelphia; schooner John W. Hall for Irvington, N. Y., with 309,000 feet of lumber and 204,000 shingles, and schooner R. T. Roundlett for Albany, N. Y., with 103,619 feet of yellow-pine lumber and 100,000 feet of cypress lumber. The Clyde steamers Algonquin and Seminole carried out about 600,000 feet of lumber and 5000 bunches of shingles to New York. The following vessels were loading here on the 26th: Schooners City of Philadelphia, City of Jacksonville, William A. Skinner, A. E. Stephens and H. J. Cottrell. Among the charters reported in New York last week were the following: A schooner, 390 tons, from Jacksonville to New York at \$4.80, and a schooner, 489 tons, from Jacksonville to New York or Portland, Maine, on private terms.

Mobile.

[From our own Correspondent.]

Mobile, Ala., September 28.

Manufacturers of lumber and timber, as well as commission men and others, begin to see a marked improvement in business in this section, and during the month about to close the demand has been steady. In timber especially a fairly active movement is now present here, and, owing to the favorable advices from Great Britain and the Continent, there is a much better feeling throughout the general market. The lumber trade is also improving, the demand from the West Indies and South America being at the moment very pronounced. Prices are still very low, but for all desirable lumber values rule firm. Sawn timber, when placed upon the market, will bring 11½ cents per cubic foot, 40-foot basis. Contracts are made on a basis of 11½ to 12 cents. Cypress is in demand at 5 to 8 cents per cubic foot, according to average. The demand for cedar is good at 25 to 35 cents, as to quality and size. In hewn timber the demand is active, and when placed upon the market will bring 11½ to 12 cents on basis of 100 cubic feet, average B1 good. Among the clearances during the last week a number of vessels are reported. The regular steamer of the Horsley Line, the Selma, went out to sea on Saturday last for Liverpool with a cargo valued at \$372,000, among which was 220,000 feet of lumber and 1200 pipe staves. The steamer Amethyst was entered and cleared for Cuba with 312,000 feet of lumber. The bark Nordsbyouman sailed for Limerick with 35,542 cubic feet of sawn timber and 13,836 feet of lumber; bark Hans for Havre with 58,285 cubic feet of sawn timber, 12,511 cubic feet of hewn timber and 11,030 feet of lumber. The bark Columbia sailed for Buenos Ayres, A. R., with 498,787 feet of lumber. Exports of other woods, in cubic feet, during the month were as follows: Oak 2466, poplar 1630, whitewood 1860, piling 2300 lineal feet, and 12,650 crossties. Freights on lumber and timber are firm, with a scarcity of tonnage offering. The following vessels are chartered to load here: Bark Geneva for Surinam at about \$7 and port charges, and bark Barbadian for Rosario at \$12 net.

Pensacola.

[From our own Correspondent.]

Pensacola, Fla., September 28.

The situation here in nearly all avenues of the lumber and timber industry is regarded as rather more encouraging than it was a month ago. Timbermen are looking forward to a fairly active

season, and prices are at present very steady, under a fair demand. Sales during the week were of two lots of timber, 1800 and 2800 pieces, respectively, both Choctawhatchie stuff, at 10 cents per cubic foot. Ferry Pass stock has been offered at a slight advance on that figure, but no sale was effected. Advices from Europe are very favorable as to prices. The Timber Trade Journal of the 19th ult., in reference to pitch pine at London, says: "The result of Messrs. Churchill & Sims's sale on Thursday were highly favorable to the maintenance of prices. All the orthodox sizes went a point higher than we last reported, and prepared boards, especially seven-eighths, showed in the rise. The balance of the pitch-pine timber ex-Everest made splendid prices; the cargo was undoubtedly a good one, but the demand has evidently gained strength as the ponds thinned off, and the prospect for this class of wood is a fair one." The movement here is at present fairly active, and the demand for lumber is increasing, especially from South American ports. This port at present, so admirably situated, is maintaining her position as one of the first lumber and timber Gulf centres, and the extensive channel improvements now in progress will greatly enhance her shipping facilities. During the past week the following vessels cleared and sailed: Bark Avenire C. for Genoa with 3780 cubic feet of hewn timber, 202,000 superficial feet of sawn timber and 309,000 feet of lumber; bark Marion Woodside for Buenos Ayres with 1,333,000 feet of lumber and 28,000 feet of pickets, and ship Pandion for London with 226,000 superficial feet of sawn timber and 786,000 feet of lumber, by Hardy & Co. The charters reported in New York last week were as follows: A British steamer, 1651 tons, hence to Holland with sawn timber on private terms; a Danish bark, 449 tons, hence to the Parana river with lumber at \$13; a bark, 453 tons, from Moss Point and Pascagoula with lumber to New York at \$5.50, and an Italian bark, 685 tons, from a Gulf port to Buenos Ayres with lumber on private terms.

Beaumont.

[From our own Correspondent.]

Beaumont, Texas, September 26.

The prevailing depression in trade has for some time affected the interior lumber market. The revival in trade predicted last spring has not yet arrived, and there seems to be no immediate tendency in that direction. The distress among the farming classes has very materially reduced demands at the yards throughout the State, and building and general improvements are stagnated.

Dealers in the interior feeling the depression have made but few orders on the mills in this section. It is, however, hoped that the recent reports of an increased cotton crop will tend to stiffen the demand for yard stock.

The shingle market is reported light, and prices reduced to below cost in some cases.

While the domestic trade is deplorably light, the export business is in good condition, and millmen all have smiling faces when foreign shipments are mentioned. Mills here are all running on full time, and have encouraging export prospects ahead.

The deep water at Sabine Pass has greatly stimulated foreign trade in this vicinity. Captain Wahl, of the ship Premier, took over 600 soundings a few days ago, and reports a depth of twenty-three feet on the bar with an average tide; the shoalest place found showed twenty-one and one-half feet of water. Before deep water was obtained, exporters here were

handicapped by the fact that they could not obtain the cheap freight rates on large cargoes that could be given at the larger ports East. Now we feel on a better footing, and trade in this section will no longer be limited to small shipments to Mexican and small Central American ports.

A practical demonstration of our possibilities will be made in a few days, when the large steamship Westmeath will arrive in Sabine Pass for a cargo of lumber from this point. She is one of the largest vessels in the lumber trade, and is said to have a capacity of 4,800,000 feet of lumber. When a vessel said to be the largest which has ever entered any Gulf port west of New Orleans comes to receive a cargo from here, we may justly feel able to compete with any of the large exporting points east of here, which have heretofore enjoyed cheaper freight rates on account of their ability to load the larger vessels.

Large rafts of timber are being towed down the river every few days to be loaded at the Pass for export, and several large bills are now being cut for shipment from the local mills to England, Germany, South America and Mexican ports, and trade outlook is better for the coming month.

The Loxley Towboat Co., of Mobile, Ala., will place a line of tugs and barges on the waters tributary to Sabine Pass about October 1, to handle the timber now being shipped to the Pass for export. This will mark a new era in methods of export, and it is expected that the expense of handling will be much reduced below the rate for rail shipments, which has been the main channel between here and the Pass heretofore.

The Reliance Lumber Co. last Tuesday booked orders for eight cargoes, aggregating 4,000,000 feet. Six of these cargoes will go to Jamaica. As the company already had several other large bills on its books, it now has sufficient orders to keep its saws busy for the next four months.

The new Hoo Hoo mill, owned by D. Tramway Call, at Call Station, on the Kansas City Railroad, is now about completed, and will begin operations within a week. Mr. J. A. Sinclair, of McArdle & Sinclair, New Orleans, has been superintending its construction. It is pronounced by millmen one of the best-equipped mills in the State.

The Orange Lumber Co., Bancroft Lumber Co. and the Litcher & Moore mills at Orange have all resumed operations, and report a fair amount of orders.

The mills north of here are all running now, and are cutting good bills of railroad timbers and ties.

It is understood that the Kansas City, Pittsburg & Gulf Railroad will soon begin dredging a channel from its terminal, Port Arthur, to Sabine Pass, and when this is completed rates will be given the mills along its line whereby they will be able to compete in the export line. On account of high freights on lumber, these mills are at present unable to get their product to the coast at a rate which would enable them to compete with the mills along the main line of the S. P. R. R.

The dry-kiln at Lake Charles, La., belonging to the Bradley-Ramsey Lumber Co., was burned last week; estimated loss \$3000.

Mr. S. A. Long is in Mexico, looking after the business interests of the "Tram."

Messrs. Mark Weiss, George Carroll and W. F. Keith are expected home from Europe in a few days.

The cypress mills of Louisiana will probably organize an effort soon to raise the price of their product. The Wm.

Cameron mill at Cameron, La., has already shut down, and others are likely to follow suit pending an advance in the market.

Mr. B. S. Woodhead, for the past three years with the Texas Tram, shook off the dust of Beaumont last Wednesday, and departed for Tampico, Mexico, where he assumes charge of the business of the Consolidated Export Co. His many friends here trust that the success here which won for him his recent appointment will attend him in his new home.

When the ship Premier clears from the Pass she will cross the bar drawing twenty-three feet of water.

Reports from the counties above here detail severe losses of timber by fire. Angelina county has suffered a loss in timber said to range from 50 per cent. in some parts to total destruction in others. The drouth has cut down the cotton about one-half.

Lumber Notes.

The German bark Marella sailed on the 23d ult. from Port Tampa, Fla., for Montevideo with 400,000 feet of lumber.

It is stated that the Bradford Land & Improvement Co. will build a new saw mill between Waldo and Fairbanks, Fla.

The Emerson Co., of Baltimore, Md., has just closed contract to install one of its drying plants at the mills of the Moore Lumber Co., of Richmond, Va.

It is stated that arrangements have been perfected for the rebuilding of the mill of the Emporia Lumber Co. at Emporia, Texas, within a very short time.

The saw and grist mill owned by Jas. Skaggs, of Lutterell, Tenn., was destroyed by fire last week. The loss is estimated at \$25,000, with no insurance.

The Texas Furniture Co., of Tyler, Texas, was chartered last week, with a capital stock of \$20,000. The incorporators are A. Harris, A. Gill and Lipshitz.

The saw mill, grist mill and gin of P. D. Vandyke, near Rome, Ga., was destroyed by fire on the 20th ult. The loss is estimated at \$2500, with insurance \$1000.

The Gardner-Lacey Lumber Co., of Georgetown, S. C., has contracted for a private system of electric lights in its mill, which will greatly facilitate operations.

The Bradley-Ramsey Lumber Co., of Lake Charles, La., sustained a \$3000 loss by the fire which destroyed its dry-kiln last week. The company will erect a new kiln at once.

The Norwegian ship Premier was loading a cargo of lumber at Sabine Pass, Texas, on the 24th ult. for a European port. She will draw twenty-three feet when ready for sea.

The Austro-American Stave Co. is making large daily shipments of oak logs and staves from Logansport, Texas. It is building barges to be used in the stave business on the Sabine river.

After a suspension of several weeks, the Virginia Manufacturing Co., of Suffolk, Va., resumed operations last week. The main products of the factory are butter-dishes and fruit packages.

The Ryan-Richards saw mill, at Lake Charles, La., is running steadily, with a fair business in prospect. The Bel-Bunker Lumber Co.'s mill, at Lake Charles, is shut down temporarily, undergoing repairs.

The H. C. McDaniel Lumber Co., of Griffin and Lonan, Ark., has changed its name to the Superior Land & Lumber Co. The old mill at Griffin will be torn down and a new one built with a capacity of 40,000 feet daily.

The receipts of cord wood at Elkton,

Md., during the past week were larger than usual, and consignments so far this season have amounted to over 4000 cords. Most of the wood comes from the poplar forests of Virginia.

The cargo of the Norwegian bark Sleipner, consisting of 929,000 feet of deals, was sold at Mobile on the 21st ult. Captain Black, of that city, was the purchaser at \$6100. The price paid was less than \$7 per thousand feet.

The Fairmont Planing Mills, at Fairmont, W. Va., were destroyed by fire on the 23d ult. Mr. J. M. Jacobs, manager of the mills, estimates the stock which was burned at \$8000. The loss is estimated at about \$20,000, with insurance \$6000.

Messrs. Hancock Bros., of Tennessee, contemplate putting up a large saw mill in Fayette, Ala., if sufficient timber can be secured. This firm expects to build a tramroad from Fayette to Newtonville, in order to get all the timber in reach of the road.

Mr. B. S. Woodhead, who has been in the employ of the Texas Tram & Lumber Co., at Beaumont, Texas, for several years, passed through Houston, Texas, on his way to Tampico, Mexico, where he is to take charge of the Consolidated Export Lumber Co.'s business.

The stock of American walnut on hand in London on August 31 was 1935 logs and 9881 planks. Notwithstanding the unusually heavy delivery of 115 logs and 194 planks during the following week, the stock on hand crept forward to 1864 logs and 11,864 planks.

The Craig Lumber Mill, at East Knoxville, Tenn., has suspended operations temporarily. The planing mill, it is stated, is still running, and the suspension of operations at the lumber mill has not affected the large sash, door and blind factory in Knoxville.

It is reported that the lumber companies along the line of the Norfolk & Western Railway, from the Ohio river to the upper edge of the timber stretch, covering a distance of 150 miles and employing several thousand men, are all preparing to make a cut in wages ranging from 10 to 20 per cent.

The British steamship Selma, the first vessel this season of the Horsley Line, left Mobile, Ala., for Liverpool on the 26th ult. The Selma's cargo consisted of 9516 bales of cotton, 4000 sacks of flour, 520 tons of pig iron, 220,000 tons of lumber and 1243 pipe staves, the entire cargo being valued at \$392,095.

It is said that an unusual demand appears to have sprung up for walnut logs for export. Millmen are making large shipments from Ashland, Ky., and hundreds of logs are going east via the Chesapeake & Ohio Railway from points on the Lexington & Eastern and Louisville & Nashville Railroads.

The receipts of lumber at St. Louis, as reported by the Lumber Exchange of that city for the week ending the 23d ult., are as follows: By rail 949 cars, and from the lower rivers 121,000 feet, compared with 1310 cars and 786,000 feet for the corresponding week last year. The shipments were 508 cars, against 756 cars a year ago.

The secretary of the Mechanics, Dealers and Lumbermen's Exchange, of New Orleans, reports the receipts of lumber for the week ending the 24th ult. at 1,695,000 feet, and for the season 10,598,000 feet, against 14,936,500 feet in 1895-96. Receipts of shingles for week 200,000, laths 165,000, oak staves 134,400 and cypress staves 34,000.

The Bancroft Lumber Co.'s mill, at Orange, Texas, started up again on the 21st ult., and will continue to run full

time on some large export orders. The Litcher & Moore Lumber Co.'s mill started up on the 22d for a steady run, and the D. R. Wingate Lumber Co.'s mill will start up on receipt of a supply of timber. All the mills of Orange will then be running, and considerable activity in lumber movements is expected.

The export business at Sabine Pass, Texas, for wood and its products is reported very active, and shipments showing a steady increase from month to month. The mills at Beaumont enjoy the export trade through Sabine Pass almost entirely, and during the past week some large orders were booked. The Reliance Lumber Co. booked six cargoes for shipment to Jamaica and two to Vera Cruz, aggregating about 4,000,000 feet. Other cargoes for South Africa, South America and Europe booked recently will keep the Reliance busy for four or five months to come.

TRADE NOTES.

The Rand Drill Co., Chicago, Ill., recently received an order to furnish the Michigan Central Railroad Co. three air compressors for its shops in Detroit, Mich., Jackson, Mich., and St. Thomas, Canada; also an order for two air compressors for the Missouri Car & Foundry Co., St. Louis.

The Shoenberger Steel Co., Pittsburg, Pa., has awarded contracts to the Ball Engine Co., Erie, Pa., and Siemens-Halske Electric Co., New York, for a complete electric-power plant, consisting of one 400 and one 150 horse-power vertical compound engine, direct connected to 225-kilowatt and 100-kilowatt generators.

During the past week large shipments of raw-hide roofing and siding paper have been made by C. S. Garrett & Son, Philadelphia, Pa., to St. Paul, Minn., Anderson, S. C., and New York city. The latter shipment was for export use. The above shows that the popularity of these products is general, and not confined to one particular locality.

The Peck-Hammond Co., of Cincinnati, Ohio, recently secured contract for heating with its warm-air system all the State Deaf and Dumb Institute buildings at Jackson, Miss. This company was awarded this contract over twenty-one bidders, embracing all systems of heating, and has reason to be proud of having secured the work under such conditions. Merit will tell.

Mr. David H. Darling, formerly general agent of the New York Belting & Packing Co., Limited, and Mr. William A. Minott, recently with the selling department of the same company, are now associated with the Mercer Rubber Co., 32 Warren street, New York city. They have the exclusive management of New York, New England and Western business of this concern, with offices at the above address.

Recent orders to furnish railway equipment, closed by Poulter & Co., Bullitt Building, Philadelphia, Pa., show that this concern is getting its share of current business. A locomotive was sent to A. A. Holbrook, Wilkesbarre, Pa., this month for use on a branch railroad now under construction. Another locomotive was shipped to Charles Kremer, at Flinton, Pa., for a new railroad developing a lumber tract.

The American Stoker Co., of Dayton, Ohio, has recently perfected a steam motor which is applied to each stoker, thus making each machine independent. This renders the work of installation very simple. It also renders the stoker practicable for use under marine boilers. This company is desirous of a general representation through engineering firms handling pumps, heaters and boiler-room supplies, and invites correspondence from interested parties.

Warm praise for the Q. & C. shop saw is given by users of this device. The Q. & C. Co., Chicago, Ill., has received some strong letters of approval of its product. One from the Haney & Campbell Manufacturing Co., Dubuque, Iowa, says: "The No. 2 Q. & C. shop saw which we purchased from you some time ago has given us the very best of satisfaction. It has been a big saving to us in our shops, and we unhesitatingly recommend said machine to any parties having machine shops as a great labor and money-saving device. We have saved, at least calculation, \$1 per day since having it in use."

Walter H. Durfee & Co., Providence, R. I., wrote "We would say in reply to your letter of the 22d inst. that we have one of your cold-sawing machines. We have used it about four months, and are well pleased with it." The Yost Manufacturing Co., Toledo, Ohio, wrote: "We are in receipt of yours of the 15th, with reference to your Q. & C. shop saws, and will say that we have one of these in use in our factory, and find it a very satisfactory machine."

Dissolution of partnership of the firm of Martinez & Maddas, Philadelphia, Pa., by mutual consent, is announced. Dion Martinez has purchased the entire interest of the firm. As engineers and contractors this firm has achieved prominence, and recently closed contract for extensive construction work in the South. We note that Mr. Martinez will continue the business, with headquarters at 1430 South Penn square, Philadelphia. The change promises to broaden the scope and resources of the business.

Much attention has been attracted to a dry-kiln installation at Glen Jean, W. Va., in the plant of the Glen Jean Lumber Co. This mill is recognized as up-to-date in all its equipment, and the enterprising president of the company, Mr. J. J. Robinson, is a believer in advanced methods. The dry-kiln plant is the compression system installed by the Emerson Co., of Baltimore. We have received a sample of quarter-sawn oak flooring dried in this kiln. It is thoroughly dried and shows a splendid finish. The sample illustrates the advantages of the Emerson system, and lumber dried by this process should find a wide demand. Favorable comment regarding the Emerson system is heard on all sides. Recently the company received from Edward E. Rueter, superintendent of the Globe Furniture Co., High Point, N. C., the following letter: "After a person has used half dozen or more of the various kinds of so-called patented dry-kilns without obtaining satisfactory results, he is calculated to become doubly cautious when desirous of making a change from former experience to another system in order to reach perfection in the drying of lumber. Such was the question with us last April, when we looked about for a method to dry lumber in a more perfect manner, and at the same time more economical in the long run. Past experience taught us to seek such results, if they could be obtained, regardless of first cost. This question now is solved, after having used your method of drying for several months. We congratulate your company on having a better method for the drying of lumber than any heretofore used by us. The kiln does what you guaranteed it would do, and it is with pleasure that we recommend its use to others in need of a first-class system."

The "Providence" steam capstans, steam gypsies and dock steam capstans, made by the American Ship Windlass Co., of Providence, R. I., are every year coming into prominence in some different line of business. Besides the many capstans made for ocean steamers, lake steamers and light-draft river steamers, for dry-docks, marine railways, for wharves, canals, etc., many are sold to railroads for wrecking cars, to manufacturing concerns for use in switching cars about their works, and within the past year a number of these steam gypsies were bought by contractors, who use them in hoisting iron beams, stone, etc., in putting up buildings, for switching cars loaded with materials, for hoisting heavy machinery, putting up engines, boilers, etc., and for various uses. Recently the company received a letter from Richard J. Baker, rigger and general contractor, Providence, R. I., who was furnished a steam gypsey for this class of work, showing what they can do. This letter is as follows: "I am pleased to say a word in favor of the steam gypsey recently purchased of you. In my business of moving and placing heavy machinery, erecting engines, boilers, etc., it has proved itself a valuable addition to my outfit. In connection with the wooden frame which I built and on which I use it, the ease and rapidity with which it can be secured in place and piped up makes it very handy. I have put the gypsey to many severe trials, and have never found it wanting; one case in particular, at the Fall River Iron Works Co.'s mill, where I hauled a 32-ton shaft up a 30-degree incline. A point of great advantage is that on account of the position of the drum or capstan you can lead it to it from any direction without changing position. I can cheerfully recommend it to parties in my line or to general contractors." A number of other contractors have spoken in high terms in regard to the saving effected by these capstans and gypsies in their business of putting up buildings and different kinds of mechanical contracts.

CONSTRUCTION DEPARTMENT.

THE MANUFACTURERS' RECORD

seeks to verify every item reported in its Construction Department by a full investigation and complete correspondence with everyone interested. But it is often impossible to do this before the item must be printed, or else lose its value as news. In such cases the statements are always made as "rumored" or "reported," and not as positive items of news. If our readers will note these points they will see the necessity of the discrimination, and they will avoid accepting as a certainty matters that we explicitly state are "reports" or "rumors" only. We are always glad to have our attention called to any errors that may occur.

* Means machinery, proposals or supplies are wanted, particulars of which will be found under head of "Machinery Wanted."

In correspondence relating to matters reported in this paper, it will be of advantage to all concerned if it is stated that the information was gained from the Manufacturers' Record.

It often occurs that the organization of a new company in any town is not known by the postmaster, and hence letters addressed to the company are returned marked "not known." The Manufacturers' Record reports the first organization of all companies, and our readers, in seeking to get into communication with them, should be very careful in deciding how to address them, and even then they must expect the return of some letters, because of the lack of knowledge on the part of postmasters of all new companies. Criticisms and complaints are invited, as they will the better enable us to guard against errors.

ALABAMA.

Birmingham—Paper-box Factory.—The Commercial Club is corresponding with New Jersey parties, who contemplate establishing a paper-box factory in Birmingham.

Gadsden—Telephone Line.—R. L. Adams has commenced the construction of a telephone line to Alabama City, Attalla and Walnut Grove.

Mobile—Coal Transportation Company.—The Mobile & Tuscaloosa Coal & Transportation Co. has been incorporated, with a capital stock of \$50,000, by Stephen Touart, C. W. Ruth, of Mobile, and F. G. Blair, of Tuscaloosa.

ARKANSAS.

Fort Smith—Water Works.—The Fort Smith Water Co. is soon to commence the improvements to its plant, which are to cost \$40,000.

Morrilton—Coal Mines.—John A. Morgan has found coal deposits near Morrilton.

Ogemaw—Lumber Mill.—The Hickok Lumber Co. expects to rebuild at once its burned mill.*

Ogemaw—Planing Mills.—The Hickok Lumber Co. will rebuild at once its mills that were burned.*

FLORIDA.

Ocala Tobacco Company.—Captain Welsh and others have organized the Welshton Tobacco & Cigar Co., to cultivate tobacco, manufacture same, etc.

Orlando—Novelty Works.—David Lockhart has bought the novelty works of L. M. Boykin and will continue the operation of same.

Wade—Saw Mill.—A. D. Wright and B. J. Potter have erected a saw mill near Wade.

GEORGIA.

Augusta—Soap Factory.—P. A. Ferris, of Atlanta, is endeavoring to establish a \$10,000 soap factory in Augusta.

Brunswick—Grain Elevator.—It is reported that the Southern Railway Co. (office, Washington, D. C.) will erect a grain elevator in Brunswick.

Chickamauga—Coke Ovens.—The Chickamauga Coal & Iron Co. is about to commence work on its additional coke ovens, recently reported. The coal mines are now working 263 men, producing over 1000 tons of coal daily. Two hundred coke ovens are to be erected at once, with coke washer and plant connected available for 800 coke ovens, to which number the plant will eventually be increased. About \$400,000 will be expended on the entire plant.

Greenway—Slate Quarries.—The Georgia Slate Co. has leased J. T. Garner's slate property and will develop same.

Macon—Iron Foundry.—Hopper & Chas. Scheell have established an iron foundry for light casting and machinery repairing.

Rossville—Electric Plant.—J. W. Stevenson can be addressed regarding the proposed electric-light and power plant.

KENTUCKY.

Georgetown—Syrup Factory.—J. C. Cantrell & Co., and W. F. De Long, of Danville, Ky., will establish in Georgetown a syrup factory.

Louisville—Distillery.—Bernheim Bros. have had plans prepared for a new distillery which they will build. A 500-bushel plant, to cost from \$50,000 to \$60,000, is contemplated.

Middlesborough—Coal Mines.—The Fork Ridge Coal & Coke Co. has struck a new coal vein, three feet wide, of cannel variety.

Nicholasville—Electric Plant.—M. A. Anderson and others are trying to organize a company to erect an electric plant.

Nicholasville—Flour Mill.—The Star Milling Co. contemplates adding some new machinery this fall.

Nicholasville—Ice Plant.—There is a movement on foot to establish ice plant. N. B. Baldwin and others are interested.

Nicholasville—Laundry.—The Monarch Steam Laundry will add some new machinery to its plant.

Nicholasville—Oil Wells.—L. C. Drake and James M. Nare have leased 20,000 acres of land in Southern Kentucky, and contemplate boring for oil.

Nicholasville—Wagon Works.—W. H. Brockman & Co. have started new carriage and wagon factory.

LOUISIANA.

Crowley—Steam Laundry.—A. B. Pickett has equipped a steam laundry.

MARYLAND.

Baltimore—Ice Plant, etc.—The Anheuser-Busch Brewing Association, of Cincinnati, Ohio, has had plans prepared for an ice plant, etc., to cost about \$75,000.

Baltimore—Construction Company.—Geo. E. Seepie, Austin McLanahan and others have incorporated the Montgomery Construction Co., with a capital stock of \$100,000.

Baltimore—Ice Plant and Bottling Depot.—The Pabst Brewing Co. (main office, Milwaukee, Wis.) has had plans prepared for the plant recently noted to be built. The buildings, including engine-house, bottling depot and 10-ton ice plant, will cost about \$75,000.

Baltimore—Electric Plant.—The City & Suburban Railway Co. will equip a plant near Catonsville to supply electric power to its Ellicott City branch.

MISSISSIPPI.

Port Gibson—Cotton Gin.—B. T. Archer will rebuild his cotton gin which was recently burned.*

Purvis—Bridge.—The county of Marion contemplates the erection of a steel bridge. Address the county clerk.

MISSOURI.

St. Louis—Brick Works.—John Shaffer, J. T. Howard, Ellis L. Evans and others have incorporated the Evans & Howard Fire Brick Co., with a capital stock of \$500,000, to manufacture fire-brick, gas retorts, sewer pipe, etc.

St. Louis—Carriage Company.—Fred Dopp, W. A. Beyerbach and others have incorporated the Zenas Varney Carriage Co., with a capital stock of \$10,000.

St. Louis—Conduit Construction, etc.—The Electric Light, Power & Conduit Co. has filed articles of incorporation, capital stock

being \$50,000. Its purpose is to construct underground conduits, etc.; incorporators, S. M. Dodd, J. C. Van Blarcom and others.

NORTH CAROLINA.

Piedmont—Cotton Mill.—The Piedmont Manufacturing Co. is making extensive improvements to its plant, including the addition of revolving flat cards, drawing frames, etc.; orders all placed.

Winston—Furniture Company.—The Huntley Furniture Co. has been organized, with \$15,000 capital, to deal in, etc.

TENNESSEE.

Hill City—Water Works.—There is talk of the construction of water works. The Chattanooga Co., Chattanooga, can be addressed for information.

Knoxville—Plano Factory.—C. B. C. Socin & Son have commenced the manufacture of planos.

Knoxville—Tinware Factory.—Hill, Lloyd & Co. will put in machinery for the manufacture of tinware. Equipment has been ordered.

Luttrell—Brick Works.—The Luttrell Brick Co. is putting new machinery in at its plant.

TEXAS.

Columbus—Cotton Company.—L. T. Pryor, Q. F. Walker and A. M. Waugh have incorporated the Stafford Land & Cattle Co., with a capital of \$10,000, to deal in land, stock, etc.

Ennis—Bridge.—There is a movement afoot to construct a wagon bridge across the Trinity river. T. D. Turner, W. W. Cochran and others are interested.

Newport—Cotton Gin.—J. M. Hedgecock will rebuild his cotton gin next season.*

Palestine—Water Works.—The city has let contract to J. W. Brynes & Co., of Galveston, for the construction of the water works recently voted for. System will cost \$90,000.

Paris—Machine Shop.—S. J. Long has completed his machine shop buildings, but has not yet bought any machinery.

San Antonio—Mercantile.—C. C. Jannin, William Aubrey and Thomas H. Clarkson have incorporated the Slayden-Clarkson Robards Co., with a capital stock of \$50,000.

Tyler—Furniture Company.—A. Harris, A. Gill and L. Lipsitz have incorporated the Texas Furniture Co., with a capital stock of \$20,000, for mercantile business.

VIRGINIA.

Orange—Telephone Line.—The Orange Telephone Co., incorporated last week, has elected George S. Shackelford, president, and W. C. Graves, secretary. The line will be constructed to Rhoadesville, twelve miles, at once.

Pocahontas—Printing Plant.—A. F. Hendricks and C. O. Echols have leased the Pocahontas Head Light printing plant, will continue the publication of the paper and improve the printing plant.

Richmond—Pump Works, etc.—Charter granted to the Sydnor Pump & Well Co. for manufacturing, etc., capital stock being \$15,000; Thomas Jeffress, president; J. C. Shafer, secretary, and T. C. Stevens, manager.

Staunton—Bicycle Works.—The Commercial Club is corresponding with the Allen-Kramer Manufacturing Co., of Danville, N. Y., relative to locating a bicycle works in Staunton.

WEST VIRGINIA.

Charlestown—Flour and Corn Mill.—Geo. W. Hyndman (until now of Hyndman, Pa.) has organized a company to erect the flour mill already mentioned. The plant is to have a daily capacity of 125 barrels; also cornmeal mill, etc.*

Dingess—Coal Mines.—The Camp Coal & Coke Co., incorporated last week, will at once open mines near Dingess.

Huntington—Oil and Coal Developments.—The Southwestern West Virginia Coal, Oil & Gas Co., recently organized, has established an office in Huntington, with R. W. H. Crabb, secretary. Developments will be probably commenced in the near future.

Keyser—Woodworking Shops.—It is said that the Baltimore & Ohio Railroad Co. (office, Baltimore, Md.) will establish wood shops at Keyser on condition that the city raise \$10,000 as a bonus.

New Martinsville—Water Works.—Caleb & Cantre, of Bridgeport, Ohio, are reported as having received contract to construct water works, to cost \$12,000.

Wetzell—Oil Wells.—W. S. Wiley, Joseph Stender and others are developing oil wells.

BURNED.

Booneville, Miss.—J. W. Brinkley's cotton gin.

Calhoun, La.—The Calhoun Tobacco Factory; loss \$3000.

Dyersburg, Tenn.—Jacob Wels's spoke factory; loss \$5000.

Ennis, Texas.—The Ennis Oil Mills destroyed by explosion; loss about \$25,000.

Fairmont, W. Va.—The Johns Bros.' Glass Works; loss \$10,000.

Fairmont, W. Va.—The Fairmont Planing Mills; loss \$6000.

Grapeland, Texas.—W. H. Thompson's cotton gin near Grapeland.

Haldes, Ga.—Jesse Wade's cotton gin; loss \$4000.

Hamilton, Ga.—O. S. Barnes's cotton gin.

Hillsboro, Texas.—The Hillsboro Ice & Cold Storage Co.'s plant; loss \$11,000.

Laurens, S. C.—J. H. Kennedy's cotton gin.

Luttrell, Tenn.—James Skagg's flour mills and lumber mills; loss about \$25,000.

Madison, Ga.—The cotton gin of John Bostwick.

Natchitoches, La.—The Givanovich Oil Co.'s cottonseed-house; loss \$5000.

New Orleans, La.—The Kory Pants Co.'s factory; loss about \$7000.

Norfolk, Va.—The Merchants & Farmers' Peanut Co.'s plant; loss \$40,000.

Quitman, Ga.—Maloy, Monroe & Co.'s turpentine still near Quitman; loss \$1500.

Rogers, Texas.—Robert Crenshaw's cotton gin; loss \$2000.

Rome, Ga.—P. D. Bandyke's cotton gin, saw mill, grist mill, etc., near Rome; loss \$2500.

Selma, Ala.—W. W. Berry's cotton gin; loss \$4000.

Shreveport, La.—Will McCormick's cotton gin.

Stark, Ga.—Jones & Redman's ginnery; loss \$2500.

Statesboro, Ga.—Ben Oliff's cotton gin near Statesboro.

Water Valley, Miss.—J. W. Simmons's cotton gin.

West Point, Miss.—Leon Chandler's cotton gin; loss \$3000.

BUILDING NOTES.

Atlanta, Ga.—Stores.—J. W. Alexander will erect four one-story stores, to cost \$5000.

Baltimore, Md.—Dwellings.—William E. Garrison will build forty-nine two-story brick dwellings; Joseph Pentz will build nine two-story dwellings; T. M. Jones will build eight two-story dwellings.

Charleston, S. C.—The Catholic Library Association will erect a building.

Chattanooga, Tenn.—George D. Lancaster will build a two-story frame house to cost \$3500.

Columbia, S. C.—Cold-storage Plant.—There is talk of the erection of a cold-storage warehouse.

Cutler, Fla.—Hotel.—It is stated that a hotel will be built.

Jennings, La.—College Buildings.—About \$25,000 will be expended on the erection of college buildings. John H. Roberts can possibly give information.

Meridian, Miss.—College Buildings.—The East Mississippi Female College will erect new buildings; Prof. J. F. Beasan, principal.

New Orleans, La.—It is stated that the Pullman Palace Car Co. will erect a building in New Orleans.

New Orleans, La.—Dwellings.—Permits issued to Leonard Smith for double cottage to cost \$1100; A. Matranza for two double cottages to cost \$8000, and to B. T. Laurence for two-story dwelling to cost \$3100.

Savannah, Ga.—Mercantile Building.—A. B. Hull & Co. will erect new mercantile buildings.

Westernport, Md.—Warehouse.—C. W. Greitzner & Son are building a feed warehouse.

Williamson, W. Va.—Courthouse.—The commissioners of Mingo county have accepted plans made by Frank P. Milburn, of Winston, N. C., for the proposed \$40,000 courthouse; to be built of buff brick, slate roof, fireproof floors, oak and marble interior, steam heat, etc. Plans will be ready in October.

RAILROAD CONSTRUCTION.

Steam Railways.

Atlanta, Ga.—A. R. Littlejohn, of New Orleans, has become interested in the project to build a road from Atlanta to Selma, Ala., which may be extended to New Orleans. R. M. Mitchell, of Atlanta, is promoting the scheme.

Baltimore, Md.—The Queen Anne's Railroad has been completed from Queenstown to a point within three miles of Denton, Md., and eight miles of track have been laid on the section from Denton to Greenwood, Del. W. H. Bosley, at Baltimore, is president of the construction company.

Baton Rouge, La.—F. M. Welch, president of the Baton Rouge & Central Louisiana road, writes the Manufacturers' Record that it will be a standard-gauge line, extending from Baton Rouge to the Texas State boundary. Surveys are now being made.

Belair, Md.—It is reported that the Stewartstown Railroad may be extended from Stewartstown, Pa., to a connection with the York Southern at Belair; also that the York Southern road may be extended from York, Pa., through Harford county to Magnolia Station, on the Philadelphia, Wilmington & Baltimore line. The extension of the Stewartstown line will be about twenty-five miles in length. J. W. Anderson, of Stewartstown, Pa., is president of the Stewartstown Railroad, and W. F. Walworth, of Cleveland, Ohio, is president of the York Southern.

Chattanooga, Tenn.—Surveys are being made for the proposed road from Chattanooga to Augusta, Ga. F. S. Wallace, of Chattanooga, has been appointed chief engineer.

Harriman, Tenn.—Mr. G. W. Chandler, president of the Harriman & Northeastern Railroad, confirms the report that this road may be extended from Petros Station to Jellico. Preliminary surveys have been made, and it is possible that the extension will be decided upon after the election in November.

Henrietta, Texas.—Prest. W. A. Squires, of the Gulf & Brazos Valley Railroad, confirms the report in the Manufacturers' Record that the company has now been organized to build this line. It is intended to run from Henrietta to Velasco, on the Gulf of Mexico, passing through Waco, Mineral Wells and Jacksboro, Texas. It will connect with the St. Louis, Oklahoma & Texas Air Line road, now under construction from Sapulpa, I. T., to Henrietta.

Hub, N. C.—The Butters Lumber Co. advises the Manufacturers' Record that its line from Hub to Fair Bluff, in the same State, is about completed. The rolling stock has also been purchased.

Memphis, Tenn.—The Illinois Central Railway Co. is relaying the section of its line between Louisville and Memphis with 75-pound steel rails; also reballasting the roadbed. J. C. Harahan, at Chicago, is vice-president in charge.

Monroeville, Ala.—S. N. Von Praag, president of the Pensacola & Northwestern Company, has been in the vicinity of Monroeville, securing right of way, etc., for the enterprise. This road will extend from Pensacola to Memphis, a distance of 440 miles.

Spartanburg, S. C.—It is reported that surveys are being made for a line from Spartanburg to Whitney, S. C., one and one-half miles. Henry Wells is the engineer. The line is to be a feeder of the Southern system.

Spartanburg, S. C.—It is reported that another company may be formed to build a railroad from Spartanburg to a point on the Carolina Central division of the Seaboard Air Line. [This has no connection with the company which has been formed to build a road between Spartanburg and Henrietta on the same system.—Ed.]

Wakita, Okla.—The Gulf Railway Co. has been chartered to construct a road from Wakita to Denison, Texas. John A. Blair, of Caldwell, Kans.; R. L. Hall and L. B. Haven, of Medford, are among the directors.

Electric Railways.

Atlanta, Ga.—It is reported that a company is to be organized for the purpose of

building an electric railroad which will connect Atlanta with Clarkston, Cumming, Decatur and Norcross, Ga.

Baltimore, Md.—The Central Passenger Railway Co. will let contracts at once for the extensions of its trolley lines in the city. George Blakiston, 44 South street, may be addressed.

De Land, Fla.—Frank E. Bond, one of the promoters of the proposed electric railroad line in this section of the State, writes the Manufacturers' Record that it is to be about twenty-six miles long, passing through Stetson, De Land, Daytona, and terminating on the Atlantic coast. The city council of De Land has granted the company right of way, and it is expected that work will be begun about December 1. It will be a single-track trolley line.

Richmond, Va.—The Richmond Traction Co. has received permission to construct an electric line on First and adjacent streets. J. Skelton Williams is president of the company.

Washington, D. C.—Baltimore and Washington capitalists have organized to build an electric line from Washington to Tacoma Park, in the suburbs. Henry W. and N. Winslow Williams, of Baltimore, are interested in the enterprise. The Montgomery Construction Co. has been formed to build the road.

Machinery, Proposals and Supplies Wanted.

Manufacturers and others in need of machinery of any kind are requested to consult our advertising columns, and if they cannot find just what they wish, if they will send us particulars as to the kind of machinery needed we will make their wants known free of cost, and in this way secure the attention of machinery manufacturers throughout the country. The MANUFACTURERS' RECORD has received during the week the following particulars as to machinery that is wanted.

Bell.—Winnsboro, S. C., wants to have a cracked bell recast. Address Jus J. Neil, town clerk.

Belting, etc.—The Hickok Lumber Co., Ogemaw, Ark., will want belting, shafting, pulleys, boxes, etc.

Boiler.—The Georgia Marble Finishing Works, Canton, Ga., will buy a boiler.

Bottles and Corks.—The Gebhardt Chili Powder Co., New Braunfels, Texas, wants to correspond with makers of bottles and corks.

Cornmeal Mill.—Geo. W. Horn, Charles-town, W. Va., will buy cornmeal mill, etc.

Cotton Gin.—B. T. Archer, Port Gibson, Miss., needs an engine, cotton gins, press, etc.

Cotton Gin.—J. M. Hedgecock, Newport, Texas, will want to buy cotton-gin equipment.

Cotton-gin Machinery.—Dan C. Crider, Kerrville, Texas, will want cotton-gin makers' materials and supplies.

Crane.—The R. D. Cole Manufacturing Co., Newnan, Ga., wants to buy a pillow crane of ten tons capacity, and a six-inch pipe-threading machine; second-hand, if in good order, preferred.

Electrical Machinery.—See "Jewelers' Tools."

Electric-light Plant.—Col. M. H. Crump, Lyndon, Ky., wants to buy electric-light plant for military academy.

Electric Motor.—E. B. Edwards, Knox Point, La., wants an electric motor to handle a hammer of about 500 pounds, to drill wells and attach to small boat.

Engine.—The Georgia Marble Finishing Works, Canton, Ga., wishes an engine.

Engine.—B. T. Archer, Port Gibson, Miss., wants an engine for gin.

Engines.—See "Hoisting Machinery."

Gas Engine.—Wanted, a gas engine, about three horse-power; must be in good condition and cheap for cash. Address, with full particulars, G 986, Sun Office, Baltimore, Md.

Grinding and Cutting Machinery.—The Gebhardt Chili Powder Co., New Braunfels, Texas, wants to correspond with makers of grinding and cutting machinery.

Heating and Ventilating Apparatus.—Proposals will be opened October 23 for installation of heating and ventilating apparatus in United States building at New Bern, N. C. Address Wm. Martin Aiken, supervising architect, Washington, D. C., for particulars.

Heating Apparatus.—Proposals will be opened October 20 for putting in heating and ventilating apparatus at United States building in Allegheny, Pa. Address Wm. Martin Aiken, Washington, D. C., for further particulars.

Heating Plant.—Col. M. H. Crump, Lyndon, Ky., wants to buy heating (steam) plant for building of forty rooms.

Hoisting Machinery.—Watkins & Hardaway, Birmingham, Ala., may possibly need one or two double-drum hoisting engines and a cableway.

Hose.—Nicholasville, Ky., will probably purchase new hose for fire department. For particulars address S. D. Young, city clerk.

Iron-works Machinery.—See "Crane."

Jewelers' Tools, etc.—E. B. Edwards, Knox Point, La., wants catalogues of jewelers' tools, parts for watches, etc.

Lumber-mill Equipment.—The Hickok Lumber Co., Ogemaw, Ark., will want to buy new or second-hand machinery for lumber mill, including flooring machines, shafting, pulleys, belting, roofing, piping, etc.

Machine Tools.—Col. M. H. Crump, Lyndon, Ky., wants to buy forge, molding machine and drawing tools for manual training school.

Machine Tools.—Dan C. Crider, Kerrville, Texas, will want a foot-power lathe. Also see "Cotton-gin Machinery."

Machine Tools.—The R. D. Cole Manufacturing Co., Newnan, Ga., wants to buy a six-inch pipe-threading machine; second-hand preferred if in good order.

Machine Tools.—C. S. Ashbrook, Louisville, Ky., wants to buy machinery for making machine and carriage bolts.

Pile Driver.—The Bonner Lumber Co., Kentwood, La., is in the market for a second-hand steam pile driver for 36-inch-gage railroad construction.

Piping and Hydrants.—The Corsicana Water Development Co., Jas. L. Autry, president, Corsicana, Texas, wants to buy twenty-five hydrants and about 5000 feet of eight-inch cast-iron water pipe.

Planing Mills.—The Hickok Lumber Co., Ogemaw, Ark., will want planers, shafting, belting, pulleys, boxes, roofing, etc.

Plating Equipment.—E. B. Edwards, Knox Point, La., wants equipment for gold plating with small battery.

Pumping Engine.—Proposals will be opened October 17 for construction of pumping engine for Kansas City, Mo., water works; capacity required, 10,000,000 gallons per twenty-four hours, with piston speed of 200 feet per minute, steam pressure 150 pounds per square inch to pump into closed mains for direct service; usual pressure 130 pounds, raised to 170 pounds for fire pressure. For further particulars address R. M. Godfrey, secretary board of public works, Kansas City, Mo.

Railroad Equipment.—The Camp Coal & Coke Co., Dingess, W. Va., will need mine cars, T steel rails, etc.

Railway Equipment.—The Bonner Lumber Co., Kentwood, La., is in the market for second-hand steam pile driver for 36-inch-gage railroad construction, caboose and three box cars.

Roofing.—The Hickok Lumber Co., Ogemaw, Ark., will want roofing.

Roofing, Siding, etc.—The G. W. Featherstone Manufacturing Co., Cedartown, Ga., wants to buy material, iron or steel, for erecting fireproof ginhouse; house to be 50x60 feet, with engine-house attached 25x30 feet; also warehouse and platforms 60x60 feet.

Saws.—The Georgia Marble Finishing Works, Canton, Ga., will need gang saws, lathes, etc.

Shafting, etc.—The Georgia Marble Finishing Works, Canton, Ga., wants shafting, pulleys, etc.

Steam Plant.—Geo. W. Horn, Charles-town, Pa., will want to buy power plant for flour mill.

Tobacco Machinery.—The Beckwith-Henderson Co., Tampa, Fla., wants to buy machinery for grinding tobacco stems.

Tools.—E. B. Edwards, Knox Point, La., wants catalogues of jewelers' tools for repairing, etc.

Water Works.—Col. M. H. Crump, Lyndon, Ky., wants to buy water fixtures for raising and distributing water through building from stream.

Woodworking Machinery.—Col. M. H. Crump, Lyndon, Ky., wants to buy wood-working machinery for manual training school.

Modern Marine Machinery.

Complete outfits in either single or twin screw, side or stern paddle wheel machinery, built by Marine Iron Works, No. 9 Dominick street, Chicago. Catalog free.

FINANCIAL NEWS.

The Manufacturers' Record invites information about Southern financial matters, items of news about new institutions, dividends declared, securities to be issued, openings for new banks, and general discussions of financial subjects bearing upon Southern matters.

No advertisements but those of a financial character will be accepted for the following page.

A Southern Man as President.

The American Bankers' Association, which is composed of representatives of the banking interests throughout the United States, has honored the South this year by electing as its president Mr. Robert J. Lowry, of Atlanta, Ga. Mr. Lowry is one of the best known men in the South, being president of the Lowry Banking Co., of that city, and interested in a number of other enterprises. He is one of the progressive, broad-minded men who have helped to make the South what it is today, and his election is a deserved tribute to his reputation and ability as a financier. The association decided to meet in Detroit in 1897. The principal business at the session was the formation of a section which is to be known as the "Section of Trust Companies," in which a large number of the trust companies of the country have enrolled themselves.

New Corporations.

The Bank of Macon, Mo., has been organized, with \$50,000 capital, by H. M. Rubey and others.

The Spring Grove Investment Co. has been incorporated at St. Louis, Mo., with \$14,000 capital stock.

The Industrial Fire & Marine Insurance Co. has been chartered to do business at Charleston, S. C.

The Tontine Loan & Security Co. has been organized at Dallas, Texas, by J. H. Stephens, J. C. Cox and others, with \$10,000 capital.

The Holland Banking Co., of Springfield, Mo., has been incorporated, with a capital of \$100,000, by C. B. and T. B. Holland and others.

The Progressive Investment Co. has been chartered to do business at Charleston, S. C. James McAlister is to be president, and John McAlister, vice-president.

New Securities.

The town of Cuthbert, Ga., will issue \$7,000 in 6 per cent. bonds. Address the town clerk.

The Suburban Railway Co., of Wheeling, W. Va., has sold \$20,000 in 6 per cent. bonds to local parties.

The General Electric Co., of New York, has taken \$61,000 of bonds issued by the city of Jacksonville, Fla., at par, less commission.

The issue of 5 per cent. bonds in the city of Norfolk, which has been placed on sale, amounts to \$25,900. Bids will be received by Hugh C. Davis and J. C. Lowenburg, chairmen of the committees interested, until October 6.

Dividends and Interest.

The Norfolk & Southern Railway Co. has declared a quarterly dividend of 1 per cent.

The Old Town Bank of Baltimore has declared a dividend of 4 per cent. for the last six months.

The Border State Building Association of Baltimore has declared a dividend of 5 per cent., semi-annual.

The Maryland Trust Co. will pay the interest due October 1 on bonds of the Pikesville, Reisterstown & Emory Grove Railway at Baltimore.

The Maryland Trust Co. of Baltimore will pay interest due October 1 on 5 per

cent. bonds of the Georgia & Alabama Railroad; also on the 6 per cent. bonds of the Chesapeake Electric & Water Co. The directors of the Portsmouth (Va.) Water Front Co. have declared a dividend of \$125 per share on the stock. This unusually large sum was the result of the recent sale of property to the New York, Philadelphia & Norfolk Railroad for railroad purposes, and the Water Front Company still has considerable property on hand.

Financial Notes.

David L. Bartlett has been elected president of the National Farmers and Planters' Bank of Baltimore, succeeding the late Enoch Pratt. John A. Whitridge has been elected vice-president.

TRADE LITERATURE.

A pamphlet descriptive of Warner's locks is sent out by the Warner Lock Co., Chicago, Ill. High-grade steel locks have won a reputation for this concern. They are finely finished, simple in construction, made of the best quality polished cold-rolled steel, and are said to be unbreakable. In their manufacture the same precision used in making high-grade gun locks is followed, with the result that they work smoothly and noiselessly. This catalogue will interest supply houses.

Modern building construction calls for a number of devices adding to the solidity and strength of the structure. One of the most useful is a steel joist hanger. Builders have sought a superior article of this class, and the appearance of the "Van Dorn" patented steel joist hanger was timely. These hangers are forged from rolled steel having a guaranteed ultimate tensile strength of 50,000 pounds per square inch. Each hanger, it is stated, will carry six times more than will be required in actual use. Simplicity and ease of application are also features. The Van Dorn Iron Works Co., Cleveland, Ohio, which manufactures these hangers, has just issued a catalogue describing them. Orders are being received by the company from all parts of the country, and the success of the new hanger seems well assured.

A revised edition of a useful book for engineers and steam users has just been published. It is edited by James P. Stanwood, M. E., and is a handy thing to have in the engine-room. The original edition was received with much favor, and this one is calculated to increase the appreciation evinced by practical men for this work. A copy can be had by anyone interested on application to Houston, Stanwood & Gamble, Cincinnati, Ohio. This concern has also issued a new catalogue of its standard slide-valve steam engines. Suggestions of value to steam users are also given in this publication, which, however, deals more with what constitutes a good engine. The two publications perform a double duty, which those who have such questions to solve will appreciate. One tells where and how to get a trustworthy and highly-efficient engine. The other gives ideas and advice as to its operation.

"A good thing is defined as an article that by its character, design, and use is of material benefit to the user. Thus a good thing in the grate line is a grate that—well, the fireman and the engineer are delighted with it, the superintendent would not have any other, and the general manager says that it saves money and fuel. It does not have to be 'pushed along.' " Experienced men in such places have thus been speaking of the grate made by the Fuller Engineering Co., Calvert, Texas. John G. Hunter, director of the power department of the Texas State Fair and Dallas Exposition Association, in a letter to the company, says: "The rocking grates you put under one of the boilers at State Fair gave the best satisfaction and worked to perfection, and where I am acquainted with most of the different makes of rocking grates in the market, believe your grate has many points of superiority. They are constructed on scientific principles, have a large percentage of air space (an essential feature), are substantial and well made, and I cheerfully recommend them to steam users as one of the best grates on the market." The Calvert Water, Ice & Electric Light Co. wrote: "We endeavored to use the common grate in burning lignite coal, which requires a good draft and care-

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ful firing. We, however, found that it was impossible to do so, and hence gave you an order for your grates, and since using same we find that this coal is very economical, and your grates enable us to burn it to a cinder, with little or no clinking. They have done more than you claim for them. To emphasize our belief, we herewith give you an order for a duplicate set for our other boiler." Some interesting literature on the subject of grates has been published by this company, and will be sent on application.

Since the publication of the last sectional catalogue of J. A. Fay & Co., Cincinnati, Ohio, some novel improvements in woodworkers have been made by this concern. These changes greatly increase the value and capacity of such machines, and are shown in a new catalogue just issued. This firm is now constructing these machines in

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two styles, distinguishing them by the words "variety" and "universal." The variety woodworkers are those that are not provided with a molder at the opposite side, but fitted with a boring and routing attachment in place of it. The universal woodworkers are those that combine the variety woodworker with a large molding machine, having capacity to work material on one, two, three or four sides. A careful study of the demands of the users of this class of machinery for range and variety of product, facility of adjustment, stability, workmanship, economy of shoproom and moderate cost, has been made. How successfully perfection has been reached by this concern in machine construction is attested in the correspondence of some of its patrons. The uses to which the woodworker can be applied approach the unlimited. Among the more important may be mentioned surfacing, planing out of wind, tapering, rabbeting, jointing, beveling, chamfering, gaining, planing, ripping, crosscutting, tenoning, squaring, raising panels, and working straight, circular or waved molding. The skill and ingenuity of the operator alone gage the variety of operations that may be performed. A detailed description of each machine is given in this publication, giving a clear insight as to the importance of these machines to manufacturers of building materials, sash, doors, blinds, furniture, pianos, wagons, carriages, street cars, railway coaches, etc. Those desiring this catalogue will be sent a copy upon application to the company.

Facilities of the International Correspondence Schools are not impaired by the recent fire in the building of the proprietors of the Colliery Engineer Co., Scranton, Pa. New and more commodious quarters have since been occupied, and a full force of instructors was at work three days after the fire. The printing plant of the school was in another building. Reserves of all instruction and question papers, drawing plates and other supplies and stationery used in the schools were in another building, and as the records of students and important files were in safes, the business is not interfered with. This system has grown to one of considerable importance in its relation to technical education, and affords advantages to those situated in localities removed from such schools to become qualified for positions of profit in various occupations.

A dispatch from Middlesborough, Ky., announces that a new vein of canal coal has been discovered near the city. It is on the property of the Fork Ridge Coal & Coke Co., and is said to be of a fine quality.

JOHN L. WILLIAMS & SONS, BANKERS,

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It would pay every town and every property-owner and agent in the South to advertise in the "Southern States" magazine. It is published by the Manufacturers' Record Publishing Co., Baltimore, Md.

PROPOSALS.

TO CONTRACTORS.

Galveston, Texas, September 17, 1896.
The Galveston Wharf Company will receive plans and specifications for the construction of a Grain Elevator of 500,000 bushels capacity, with an estimate of the probable cost of constructing same on its property in Galveston, Texas, until November 14, 1896. The Wharf Company reserves the right to reject any or all plans submitted, and should one be selected all contractors will be permitted to bid on construction of Elevator according to plans selected.

Also plans and specifications for a Cleaning House, with a capacity of 250,000 bushels, subject to above conditions.

For further information address

GALVESTON WHARF COMPANY,
JOHN E. BAILY, Secretary.

UNITED STATES ENGINEER OFFICE,
106 Granby Street, Norfolk, Va., September 9, 1896.—Sealed proposals for dredging Western Branch of Elizabeth River, Va., will be received here until 12 o'clock October 26, 1896, and then publicly opened. Information furnished on application. Address THOS. L. CASEY, Capt., Engineers.

TREASURY DEPARTMENT, Office Supervising Architect, Washington, D. C., September 18, 1896. Sealed proposals will be received at this office until 2 o'clock P. M. on the 9th day of October, and opened immediately thereafter, for all the labor and materials required for the approaches to the U. S. Courthouse and Postoffice, Martinsburg, W. Va., in accordance with the drawing and specification, copies of which may be had at this office or the office of the Custodian at Martinsburg, West Virginia. Each bid must be accompanied by a certified check for a sum not less than two per cent. of the amount of the proposal. The right is reserved to reject any or all bids and to waive any defect or informality in any bid, should it be deemed in the interest of the Government to do so. All proposals received after the time stated for opening will be returned to the bidders. Proposals must be enclosed in envelopes, sealed and marked, "Proposal for the Approaches to the U. S. Courthouse and Postoffice Building at Martinsburg, W. Va.," and addressed to WM. MARTIN AIKEN, Supervising Architect.

TREASURY DEPARTMENT, Office Supervising Architect, Washington, D. C., Sept 19, 1896. Sealed proposals will be received at this office until 2 o'clock P. M. on the 20th day of October, 1896, and opened immediately thereafter, for all the labor and materials required for the low-pressure, return circulation, steam heating and ventilating apparatus for the U. S. Postoffice building at Allegheny, Pa., in accordance with the drawings and specification, copies of which may be had at this office or the office of the Superintendent at Allegheny, Pa. Each bid must be accompanied by a certified check for a sum not less than two per cent. of the amount of the proposal. The right is reserved to reject any or all bids and to waive any defect or informality in any bid, should it be deemed in the interest of the Government to do so. All proposals received after the time stated for opening will be returned to the bidders. Proposals must be enclosed in envelopes, sealed and marked, "Proposal for the Heating and Ventilating Apparatus for the U. S. Postoffice building at Allegheny, Pa.," and addressed to WM. MARTIN AIKEN, Supervising Architect.

TREASURY DEPARTMENT, Office Supervising Architect, Washington, D. C., September 25, 1896. Sealed proposals will be received at this office until 2 o'clock P. M. on the 28th day of October, 1896, and opened immediately thereafter, for all the labor and materials required for the interior finish of basement, first story, etc., of the U. S. Postoffice, Washington, D. C., in accordance with drawings and specification, copies of which may be had at this office or the office of the Superintendent at Washington, D. C. Each bid must be accompanied by a certified check for a sum not less than two per cent. of the amount of the proposal. The right is reserved to reject any or all bids and to waive any defect or informality in any bid, if it be deemed in the interest of the Government to do so. All bids received after the time stated will be returned to the bidders. Proposals must be enclosed in envelopes, sealed and marked, "Proposal for the Interior Finish, etc., for the U. S. Postoffice, Washington, D. C.," and addressed to WM. MARTIN AIKEN, Supervising Architect.

TREASURY DEPARTMENT, Office Supervising Architect, Washington, D. C., September 28, 1896. Sealed proposals will be received at this office until 2 o'clock P. M. on the 29th day of October, 1896, and opened immediately thereafter, for all the labor and materials required for the low pressure, return circulation, steam heating and ventilating apparatus for the U. S. Postoffice, Courthouse and Custom-house building at Newbern, N. C., in accordance with the drawings and specification, copies of which may be had at this office or the office of the Superintendent at Newbern, N. C. Each bid must be accompanied by a certified check for a sum not less than two per cent. of the amount of the proposal. The right is reserved to reject any or all bids and to waive any defect or informality in any bid should it be deemed in the interest of the Government to do so. All proposals received after the time stated will be returned to the bidders. Proposals must be enclosed in envelopes, sealed and marked, "Proposal for the Heating and Ventilating Apparatus for the U. S. Postoffice, Courthouse and Custom-house Building at Newbern, N. C.," and addressed to WM. MARTIN AIKEN, Supervising Architect.

TREASURY DEPARTMENT, Office Supervising Architect, Washington, D. C., September 28, 1896. Sealed proposals will be received at this office until 2 o'clock P. M. on the 29th day of October, 1896, and opened immediately thereafter, for all the labor and materials required for the stone and brickwork, roof covering and interior finish above second story (except plumbing, gas piping, heating apparatus, elevators and electric wiring) of the U. S. Appraiser's Warehouse at New York, N. Y., in accordance with drawings and specification, copies of which may be had at this office or the office of the Superintendent at New York, N. Y. Each bid must be accompanied by a certified check for a sum

not less than two per cent. of the amount of the proposal. The right is reserved to reject any or all bids or to waive any defect or informality in any bid should it be deemed in the interest of the Government to do so. All proposals received after the time stated will be returned to the bidders. Proposals must be enclosed in envelopes, sealed and marked, "Proposal for Stone and Brickwork, Roof Covering and Interior Finish, etc., for the U. S. Appraiser's Warehouse at New York, N. Y.," and addressed to WM. MARTIN AIKEN, Supervising Architect.

HELP WANTED.

Advertisements under this head are inserted free of charge for readers of the MANUFACTURERS' RECORD. Advertisements must not exceed five lines—about thirty words—and four insertions will be given without charge.

WANTED—Active, reliable man to sell one hundred shares installment stock in best Southern manufacturing enterprise, Atlanta, Ga. None similar in the South. Will earn thirty to forty per cent. Pay for this work then permanent job. Address SECRETARY, care Manufacturers' Record. 09

WANTED—To correspond with man experienced in manufacture of wheelbarrows with view to taking charge of new plant on small scale. Address F. care Mfrs. Record. 09

SITUATIONS WANTED.

Advertisements under this head are inserted free of charge for readers of the MANUFACTURERS' RECORD. Advertisements must not exceed five lines—about thirty words—and four insertions will be given without charge.

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WANTED—Position by a Viennese as representative of exporters to Germany, Austria and Hungary; will be residing in Vienna, Austria, where he has lived for twenty-three years; can furnish best references. Address B. FUCHS, 1105 W. Chicago av., Chicago, Ill. 023

WANTED—A position as draughtsman, chief assistant or superintendent of office work, or on building; twenty years' experience and best of references. Address DRAUGHTSMAN, care Manufacturers' Record. 016

AFTER ten years' experience in the machinery line in the United States, I will return to Paris, France, on account of my health; I solicit the patronage of American manufacturers, so as to represent them now and during the grand Exposition; I shall give the best references as to honesty, ability and good judgment. Address P. O. Box 1757, New Orleans. 016

POSITION—By reliable young man well acquainted with the South, position with transportation company or investment corporation; accustomed to managing; also good accountant and correspondent; excellent references. Address G. R. F., care Mfrs. Record. 016

WANTED—Position as salesman, receiving or shipping clerk in hardware store dealing in mill, railroad and steamboat supplies, or store dealing in machinery; thoroughly qualified to take complete charge of such a store; best of reference. Address W. R., care Mfrs. Record. 016

YOUNG MAN, practical, experienced and steady, first-class bookkeeper, stenographer, typewriter rapid and accurate—wants good position in the South or Southwest; capable of taking entire charge of correspondence without dictation; not afraid of hard work; reliable references; at present holds responsible position. Address PRACTICAL, P. O. Box 66, Staunton, Va. 016

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WANTED—A position as superintendent of a sash, door and blind factory; young man with fifteen years' experience. Address 119 Lock Box, Hickory, N. C. 02

Wanted—An Idea Who can think of some simple thing to patent? Protect your ideas; they may bring you wealth. Write JOHN WEDDERBURN & CO., Patent Attorneys, Washington, D. C., for their \$1.00 prize offer and list of two hundred inventions wanted.

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U. S. MAIL CHUTES

WHICH ARE
A NECESSITY IN OFFICE BUILDINGS AND HOTELS.
Write to the sole makers,
THE CUTLER MFG. CO., Rochester, N. Y.
PATENTED. AUTHORIZED.

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INTERESTED IN THE
SOUTH?

If so, you cannot afford to miss reading the MANUFACTURERS' RECORD every week. We want your subscription. Price \$4.00 a year. Subscribe TO-DAY.



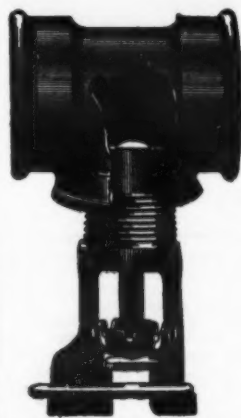
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Enhance the Value of Property One-Third.
DURABLE HEALTHFUL NOISELESS
20,000,000 square yards laid in eighteen years.

For plans and estimates apply to

THE BARBER ASPHALT PAVING COMPANY,
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PUECH & FRERET,
634 Gravier St., NEW ORLEANS, LA., U. S. A.
Cotton Seed Products. TANK CARS FREE.
EXPORTERS OF GRAIN. **Oil Mill Supplies.**



Absolute Protection from

. FIRE .

OBTAINED by the Use of

The "Non-Corrosive" Automatic
Sprinkler System.

It will reduce your insurance from 25 to 50 per cent. Endorsed by the Eastern mutuals and all stock insurance companies. We can furnish facts on this subject of value to you.

MANUFACTURERS' AUTOMATIC SPRINKLER CO.

136 Liberty Street, New York City.

FIRE INSURANCE.

H. L. White & Co., 19 Liberty Street, - - - New York City.

Insurance placed on all classes of property. References furnished.

Value Received



every time, say buyers of the....

NUMBER **SIX** MODEL
Remington
Standard Typewriter

Value in Work Done; in Time Saved;
in Convenience—always ready;
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Send for Illustrated Booklets.
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Moderate terms. Before applying for a
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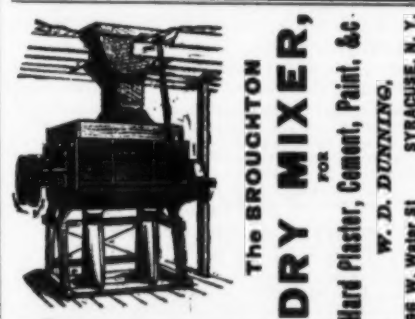
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THE UNION CENTRAL
LIFE INSURANCE COMPANY

of Cincinnati, Ohio.
Assets over \$13,000,000.

"One of the most pushing, thriving life companies in the land."—Ins. Age, N. Y. City.
Highest interest rate. Lowest death rate.

Safest investments.
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President. Secretary.
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PRESS

FOR BALING
Cloth, Paper, Yarn, &c.
Or for any other purpose re-
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It is my business to
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OPPORTUNITIES FOR INVESTORS.

OWNERS of 600 Acres of elevated land adjoining the suburbs of the City of Shreveport, Louisiana, have given to MR. ANDREW CURRIE an option on the same at very low prices, to induce industrial enterprises to invest and locate. The city handles 100,000 bales of cotton, and its new railroad connections tap extensive forests of pine of which MR. CURRIE has 30,000 acres for sale, interspersed in the glades with gum, beech, hickory, holly, magnolia and a variety of oak. Address
A. CURRIE, Shreveport, La.

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Abstracts furnished. Titles Insured. Property managed and handled for non resident owners. Examinations and reports made as to the value and condition of lands. Timber lands bought and sold. Capital, \$50,000.

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For Sale Several Fine Residences and Choice Building Lots.

An ideal city for a winter home, full of historic interest, and a climate soft and mild in winter, but not hot enough to be enervating. Cost of living is cheap.

The City of Charleston's water supply is obtained from artesian wells, and it has recently been proven to be a valuable dyspeptic cure when used as drinking water, and a rheumatic cure when bathed in hot.

Also Farming Lands near Charleston, S. C., suitable for market gardening, which is a large and profitable industry in the vicinity of Charleston. For further particulars address

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— TO CLOSE AN ESTATE.

Almost new, Engine, Boiler, Band Mill, two Gangs, Lath and Picket Mill, Trucks, etc.
Also 6000 feet T Rails.
Sold at a bargain on very easy terms.
Address

WOOLVERTON & TINSMAN,
WILLIAMSPORT, PA.

Glass Factory

FOR SALE.

Under and by virtue of the authority vested in the undersigned trustees by that certain deed of trust, executed by the HUNTINGTON GLASS COMPANY, bearing date on the 1st day of March, 1894, and recorded in the Clerk's Office of the County Court of Cabell County, in trust deed book number thirty (30) at page one hundred and thirty-one (131) et seq., default having been made and continued for six months in the payment of the interest coupons, and having been so requested in writing by a holder of some of the bonds, secured by said trust deed so to do, the undersigned will on the

EIGHTH DAY OF OCTOBER, 1896,

at 2 o'clock P. M., on the premises, situate in Central City, Cabell County, West Virginia, sell to the highest bidder, all the property, real and personal, franchises and liberties in said trust deed conveyed, and therein described as follows: "All that certain piece or parcel of land situate in Central City, Cabell County, West Virginia, bounded on the north by Virginia Avenue, on the east by Fourteenth Street, on the south by Washington Avenue, on the west by Fifteenth Street, which said lot of land is designated on the official map of said Central City, made by George McKendree, Engineer, a copy of which was filed in the Clerk's Office aforesaid on the 31st day of October, 1891, as block number ninety-four (94), being four hundred feet by three hundred and thirty feet, together with all and singular the buildings, warehouses and shops, railroad switches, together with all the furnaces, gas producers with their connections, mold shop, machinery and tools, office furniture and fixtures, presses, fixed machinery and miscellaneous property, consisting of tools and other machinery used in the manufacture and selling of glassware and composing the present plant of said glass company."

TERMS OF SALE.

One-third of the purchase money thereof cash in hand on day of sale, and the balance in two equal instalments at six and twelve months, the purchaser executing his notes with good personal security for the deferred payments, bearing six per cent. interest.

The legal title of said property will be retained as further security for the deferred instalments of the purchase money.

Given under hands as trustees this 5th day of September, 1896.

C. W. CAMPBELL,
JOHN H. HOLT,
Trustees.

NOTICE.

The dissolution of the GEORGE PLACE MACHINE CO., through a Receiver, in no way affects me or my regular business, as I had no financial interest in the COMPANY. My name has never been, nor is it now, on any of its obligations.

Owing to the Corporate name under which the Company has been doing business, I thought it best to make this explanation.

Very respectfully,

GEORGE PLACE.

New York, September 19, 1896.

MANUFACTURERS' attention is called to the large six-story Central Power Station of the Capital Traction Company, covering an entire block in the very centre of the business section of the nation's capital city. Suitable space for light manufacturing plants and other business, with power, heat and electric light, can be obtained at moderate rates. Address

CAPITAL TRACTION COMPANY,
Washington, D. C.

COAL 2000 acres on railroad and waters of Monongahela River, near Clarksburg, W. Va., consisting of PKTSburg vein 9 feet thick. Cannot be excelled for coke or gas. Perfect location. Very low freight rate. Can be bought cheap.

TIMBER 55,000 acres of finest timber land in Western North Carolina. Expert estimates that it will cut 75,000,000 feet of merchantable lumber. Title absolutely perfect. Guaranteed to come up to representations. The cherry timber alone will almost pay for the land. Price very low.

Territory in Harrison, Lewis and Gilmer counties, West Virginia, ready for development. Want operators to put down test wells. We have fine tracts of coal, timber and farm lands in addition to the above in W. Va., Va., Ky. and Tenn. Full particulars on application. Address THE SOUTHERN REAL ESTATE EXCHANGE, Clarksburg, W. Va.

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Farms, Timber Lands, Water Powers and choice City Property in the fertile Catawba Valley among the Carolina Mountains. Write for illustrated pamphlet, "Catawba Valley and Highlands."

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MORGANTON, N. C.

While Others Hesitate You

CAN OBTAIN A HOME IN SOUTHWESTERN LOUISIANA

Where Real Estate has increased 500 per cent. in the last few years. If you contemplate going South, why not go where there is no over-production of agricultural products, there is no over-production of Sugar or Rice, the two principal crops of this section.

Choice fruits and early vegetables always bring good prices, especially if placed on the market of the world when other sections of the country are frozen up.

If you want to enjoy perfect health and live where all the conditions of life are most favorable, then seek the Coast Country of Southwest Louisiana, of which the town of CROWLEY is the centre.

A FINE STOCK COUNTRY, RICH FRUIT LANDS well adapted to the growing of Hay, Oats, Corn, Cotton, Rice, Sugar-cane and all of the semi-tropical fruits. The opportunities that are there offered are fast being taken up. If you will write to

W. W. DUSON & BRO.

CROWLEY, LA., they will send you a map of this section and their new book, "Come and See," which will furnish you full information about this favored land.

....FOR SALE....

20,000 trees WHITE and RED OAK, from 24 inches to 5 feet in diameter.
20,000 acres of RED and BLACK CYPRESS.
118,000 acres of LONG LEAF YELLOW PINE.
25,000 acres of ELM TIMBER. Address

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GOLD MINES?
MICA MINES?
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TIMBER LANDS?
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WRITE ME.

I have lots of such property for sale ranging from 100 to 11,200 acres in extent.

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Business men desiring to open offices in Baltimore will find in the

MANUFACTURERS' RECORD
BUILDING

superior advantages in location, comfort and convenience.

The building is modern in every respect; furnished throughout in quartered oak; steam heat, electric and gas lights, quick elevator service, mail chute, etc.

LOCATION,

corner Lexington and North streets diagonally opposite the City Hall and Postoffice, and immediately facing the temporary Courthouse which will be occupied for the next five years. All the principal street car lines pass within one square, giving quick access to every part of the city.

Considering the location, conveniences and services, these are the cheapest offices in Baltimore.

Light, heat and janitor service free
Inspection invited.

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WAS ESTABLISHED IN 1849.

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Baltimore Office, Equitable Building,
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FOR SALE.

Works of the Phosphate Mining Co., Limited

Under order of the
United States Circuit Court for the District of
South Carolina.

The valuable piece of property, being the works of the Phosphate Mining Co., Limited, generally called Brotherhood's, situated about 1½ miles from Port Royal, S. C. Consisting of about 24 acres, more or less. Having a river frontage on Battery Creek of 971 feet, with fine wharves, etc. Convenient for loading ocean steamers (have from this point carried down steamers loaded to 21 ft. 6 in.) The Port Royal & Augusta Railroad passes through the property and has suitable switch conveniently located.

On property is fine large open shed some 340 feet by 70 feet, brick piers, with three railroad tracks overhead. Other desirable warehouse buildings, with overhead railroad trestles from wharves, boiler-house, etc. Desirable dwelling-houses and outhouses. Fine artesian well and large brick cisterns.

A most desirable site for Cotton Mill, Manufacturing, Warehouse purposes, Ocean Shipments.

For particulars apply to

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For sale under order of United States Circuit Court for South Carolina.

The powerful elevator dredge John Kennedy, recently in use dredging South Carolina river phosphate rock.

Can be readily converted for ordinary dredging purposes, working in from 10 feet to 42 feet of water.

Especially constructed for dredging very hard material. Can be seen at Phosphate Mining Co., Limited, works near Port Royal, S. C.

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F. BROTHERHOOD, Receiver,

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A number of 120-ton LIGHTERS, very strongly built and in good condition.

One 60 horse-power Return Tubular Boiler, complete, with independent fronts, smoke stack and fittings.

One Head & Sisco Centrifugal Pump, 10-inch discharge, with vertical engine driving direct. (Rice planters' attention called to above.)

A number of Williamson Friction Hoisting Engines.

One Steam Crane to lift three ton; radius of jib 15 feet.

One Steam Travelling Crane to lift ten ton; radius of jib variable up to 30 feet; gauge of track 6 feet; complete, with boiler and water tank.

One Blake Steam Pump, 10x12x12; suction 6 inch, discharge 4 inch.

One Blake Steam Pump, 4½x3½x8; suction 1½ inch, discharge 1 inch.

Two Blake Plunger Feed Pumps, 5½x3½x7; suction 2 inch, discharge 1½ inch.

A Small Air Compressor.

Steam Cylinder 3½ in. dia., 6 in. stroke.

Air Cylinder 3½ in. dia., 6 in. stroke.

Also a Rivetted Steel Receiver for above, 30 in. dia. by 4 ft. Has been used in connection with Lucigen Light, but as good as new.

One Phosphate Rock Tub Washer, complete, with shafting, gears, etc., and duplicates.

Complete Blacksmith Shop outfit.

Two Steel Cylinder Washers for phosphate rock, complete, with rollers and gearing.

Two pair of powerful Crusher Rolls for phosphate rock, complete, with pulleys and gearing.

Two Direct Acting Steam Elevators, with carriage, etc.; steam cylinder 15 inch by 17 feet 9 inch lift, and steam cylinder 20 inch by 16 feet 9 inch lift.

Pile Driver Frame, 1800-pound hammer and fixings.

Punching Machinery, one H. C. Pond Co.'s make, one Watson & Stillman, No. 5.

Artesian Well Tools, Platform Scales from 200 pounds to 16,000 pounds.

Eight good Rock Field Cars, 3 foot gauge; two Dumping Frames for ditto.

Dry Shed Side Dumping Cars, 3-foot gauge, Shafting, Pulleys, etc.

Assorted Piping and Fittings, etc.

Steam and Water Hose and valuable Tools, Materials, etc., etc.

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One new ten-wheel freight engine, six-wheel connected, built by Cooke Locomotive Works, weight 52 tons.
To rent—Two Baldwin passenger engines. All standard gauge and in first-class condition.
Address
CHATTANOOGA SOUTHERN R. R. CO.
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For Sale Cheap.

3 36-in. Gauge Locomotives.
48 36-in. " Freight Cars.
3 36-in. " Passenger Coaches.

DAVIS, KELLY & CO., Louisville, K**FOR SALE.****RAILS and ROLLING STOCK**

2000 tons 30, 36 and 40-lb. Steel Relaying T Rails.
10 BOX Coal Cars 40,000 pounds capacity;
10 FLAT Standard Gauge; good condition.
Also lot Narrow Gauge Engines and Cars.
Call on or write

BLOCK-POLLAK IRON CO.

CINCINNATI, OHIO, or CHICAGO, ILL.

We are always in the market to buy Relaying Rails and Scrap Iron of all kinds for cash.

FOR SALE RELAYERS.**DALLETT & CO.**

201 Walnut Place, PHILADELPHIA.

Buyers of Scrap Iron and Steel. Sales agents for Pioneer Mining & Mfg. Co. of Alabama, Bessemer Iron. High grade North Carolina Ores.

FOR SALE.

EIGHT MILES 30-lb. STEEL RAILS, as good as new.

One Standard Narrow Gauge Locomotive.
" " " Passenger Coach.
" " " Combination Coach.
Two " " " Box Cars.
" " " Flat Cars.

SABEL BROS., JACKSONVILLE, FLA.**NEW and RELAYING TEE RAILS****FOR SALE.**

About 135 tons new 35-lb. Steel Tee Rails, N. Y. delivery. About 250 tons new 40-lb. Steel Tee Rails and Angle Joints, Pittsburgh delivery. About two miles relaying 20-lb. Steel Tee Rails, Northern N. Y. delivery. About 35 tons of relaying 25-lb. Steel Tee Rails, South Carolina delivery.

THE STEEL RAIL SUPPLY CO.

100 Broadway, NEW YORK, N. Y.

LOGGING CARS,
LIVE ROLLS,
DRY KILNS,
IRON KILN TRUCKS

SECOND-HAND LOCOMOTIVES**UNION IRON WORKS CO.,** Selma, Ala.

Either for Cash

SPECIAL SALE

Or on Easy Terms.

Excellent Lot of

HEAVY PASSENGER and FREIGHT LOCOMOTIVES

and First-Class

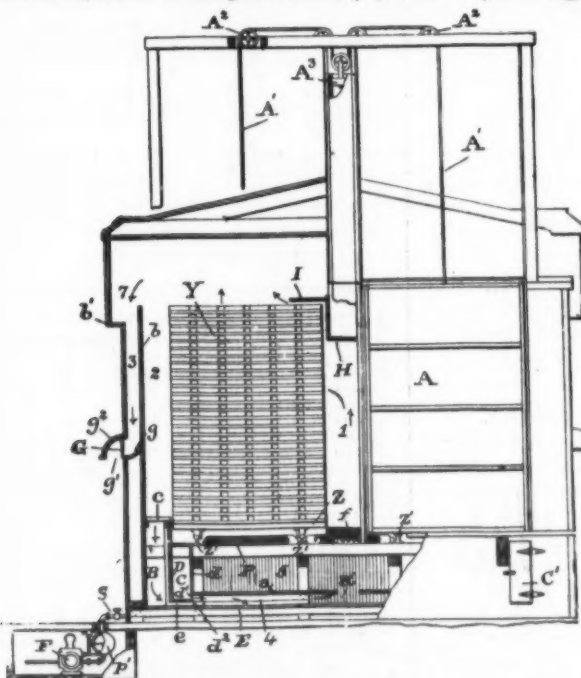
PASSENGER, COMBINATION and FREIGHT CARS

Selected by our own experts from amongst some of the finest lots of Second-hand Equipment that have ever been offered by any of the Leading Trunk Lines of this country. The larger part of the Passenger Cars are Equipped with Four and Six-Wheeled Trucks, Westinghouse Air Brakes, Janney and Miller Couplers, Plush Seats, Spear Heaters, Bronze Trimmings and all modern improvements. For Specifications, Photographs, Prices and Terms address

NEW YORK EQUIPMENT CO., 80 Broadway, N. Y.**Do You Want to DRY...**

Your LUMBER at the least expense and with the least attention?

THEN USE THE

EMERSON PATENTED Automatic Compression Moist Air System of Drying.

WE GIVE an unequalled guarantee as to quality and quantity of material dried and economy in the use of steam.
It Costs Less Money
Requires Less Building
Does More Perfect Work

Than any known system of drying.
Our catalogue illustrates specimens of wood dried by our Moist Air Process, with views and arrangements of the leading plants in the United States that are using the system, and will interest any one.

OUR AUTOMATIC COMPRESSION SYSTEM is adapted to the drying of all forest products, and is meeting with great favor with the Furniture Manufacturers for drying oak.

OUR STEEL ROLLER BEARING TRUCKS are made for heavy use, and their perfect bearings make them run light. If you are interested in drying lumber, or any other material, write us.

Suit for infringement has been filed in the U. S. Court for the Eastern district of North Carolina, which will be prosecuted to the fullest extent of the law.

THE EMERSON COMPANY,

809 Fidelity Building,

BALTIMORE, MD.

Manufacturers

CABINET AND BUILDERS' HARDWARE

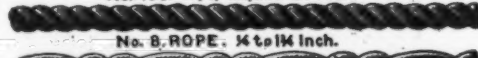
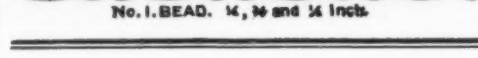
and Specialties in Iron.

Wrightsville Hardware Co**WRIGHTSVILLE, PA.**

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SHEPARD HARDWARE CO., Buffalo, N. Y.

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**Blind Hinges, Gate Hinges, Gate Latches and Frame Pulleys**No. 57. $\frac{1}{2}$, 1, 1 $\frac{1}{4}$ and 1 $\frac{1}{2}$ inch.No. 42. $\frac{1}{2}$, $\frac{3}{4}$, 1 and 1 $\frac{1}{2}$ inch.No. 21. $\frac{1}{2}$, $\frac{3}{4}$, 1, 1 $\frac{1}{4}$ and 2 inch.No. 136. $\frac{1}{2}$, 1, 1 $\frac{1}{4}$ and 1 $\frac{1}{2}$ inch.No. 8. ROPE. $\frac{1}{2}$ to 1 $\frac{1}{4}$ inch.No. 1. BEAD. $\frac{1}{2}$, $\frac{3}{4}$ and 1 inch.**HIGHEST AWARD****COLUMBIAN****EXPOSITION****ON CARVED MOULDINGS.**

VARIETY OF PATTERNS,
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CLEAN, PERFECT WORK.

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Grand Rapids Carved Moulding Co.**GRAND RAPIDS, MICH.****LOG CARTS.**

We build them RIGHT.
We build them CHEAP.

SOUTHERN LOG CART & SUPPLY CO.

Down in
MOBILE, ALA.

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Manufacturer of all kinds of

KILN-DRIED and JOINTED STAVES A

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Sawed on the Log, for Domestic or Foreign Trade.

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Treated with Dead Oil of Coal Tar.

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LOOK CAREFULLY through this paper and you may find an advertisement of some machinery or other goods that you are in need of. Preserve this copy for future reference.

SOUTHERN LUMBER DIRECTORY.

A List of Leading Lumber Dealers and Manufacturers in the South.

This list of representative Southern lumber merchants and manufacturers is published for the benefit of those who desire to reach responsible houses in this branch of business in the South. Readers of the MANUFACTURERS' RECORD who have occasion to correspond with any of the firms mentioned below will confer a favor by mentioning this paper.

YELLOW PINE.
Manufacturers.

Villa Rica Lumber Co., Anniston, Ala.
Reddie & Simonson, Birmingham, Ala.
Hawkins & Smith, Birmingham, Ala.
Marbury Lumber Co., Bozeman, Ala.
Marbury & Jones, Bozeman, Ala.
Peters Lumber Co., Brewton, Ala.
W. T. Smith Lumber Co., Chapman, Ala.
LaFayette Cooke, Cook's Springs, Ala.
Dunham Lumber Co., Dunham, Ala.
W. J. Williams & Son, Eustis, Ala.
E. Walker, Flomaton, Ala.
Gadsden Lumber Co., Gadsden, Ala.
Tuscaloosa Lumber Co., Hull, Ala.
J. B. Adams, Longview, Ala.
S. B. Allen & Co., Montgomery, Ala.
Moore & Kirkland, Montgomery, Ala.
Wm. C. Holt, Montgomery, Ala.
Pansy Lumber Co., Pansy, Ala.
T. H. Boxley, Sterrett, Ala.
S. A. Blassingame, Verbena, Ala.
J. C. Jackson & Sons, Wilsonville, Ala.
Martin Lumber Co., Bollinger, Ark.
Long-Bell Lumber Co., Buckner, Ark.
T. M. Turple, Caldwell, Ark.
Cotton Belt Mill, Cotton Belt, Ark.
Fardyce Lumber Co., Fardyce, Ark.
Fort Smith Lumber Co., Fort Smith, Ark.
Red River Lumber Co., Lewisville, Ark.
North Arkansas Lumber Co., Portia, Ark.
Stiff City Lumber Co., Pine Bluff, Ark.
Kansas City & Southern Lumber Co., Sedgewick, Ark.
J. I. Porter Lumber Co., Stuttgart, Ark.
A. J. Neimeyer Lumber Co., Waldo, Ark.
Simpson & Co., Bagdad, Fla.
Carey & Ollinger, Bagdad, Fla.
Sanford Lumber Co., Caryville, Fla.
Skinner & McDavid, Escambia, Fla.
J. P. Little, Sumner, Fla.
Chipola Lumber Co., Mariana, Fla.
Southern States Land & Lumber Co., Muscogee, Fla.
Armstrong & White, Neoga, Fla.
McMillan Mill Co., Pine Barren, Fla.
Georgia & Florida Investment Co., Tallahassee, Fla.
Perkins Mfg. Co., Augusta, Ga.
Flint River Lumber & Export Co., Bainbridge, Ga.
Brown & Garber, Brunswick, Ga.
Donaldson Lumber Co., Donaldsonville, Ga.
T. W. Garbutt & Co., Garbutt, Ga.
James K. Clark Lumber Co., Gertrude, Ga.
Bewick Lumber Co., Hazelhurst, Ga.
O'Neill Manufacturing Co., Rome, Ga.
Hilton & Dodge Lumber Co., St. Simon's Mills, Ga.
Julius Levin & Co., Alexandria, La.
Bradley-Ramsey Lumber Co., Lake Charles, La.
Gulf Lumber Co., New Orleans, La.
W. W. Carro, New Orleans, La.
McEwen & Murray, Limited, New Orleans, La.
A. Wilbert's Sons Lumber & Shingle Co., Plaquemine, La.
Hearn & Co., Robertsville, La.
Victoria Lumber Co., Victoria, La.
Keystone Lumber & Imp. Co., Bogie Chitto, Miss.
J. S. Blackburn, Ellisville Depot, Miss.
W. L. Rankin & Bro., Fayetteville, N. C.
C. C. Wade & Son, West End, N. C.
Bridges & McArthur Lumber Co., Wilmington, N. C.
D. W. Alderman, Alcolu, S. C.
E. D. Mins, Edgefield, S. C.
Mallard Lumber Co., Greeleyville, S. C.
Bellance Lumber Co., Beaumont, Texas.
Lutcher & Moore Lumber Co., Orange, Texas.
D. R. Wingate Lumber Co., Orange, Texas.
Southern Pine Lumber Co., Texarkana, Texas.
Warren Lumber Co., Warren, Texas.
Wm. Kirk, Parkersburg, Va.
Cairo Mfg. & Lumber Co., Petroleum, W. Va.

NORTH CAROLINA PINE.
Manufacturers.

Samuel Eccles, Jr., Baltimore, Md.
Geo. F. Sloan & Bro., Baltimore, Md.
Kryland & Brooks Lumber Co., Baltimore, Md.
R. T. Waters & Son, Baltimore, Md.
Aberdeen Lumber Co., Aberdeen, N. C.
Page Lumber Co., Aberdeen, N. C.
C. W. Mobley & Co., Berry, N. C.
Gay Manufacturing Co., Bosley, N. C.
Albemarle Lumber Co., Elizabeth, N. C.
W. L. Rankin & Bro., Fayetteville, N. C.
Frank H. H. Hamilton, N. C.
Batters Lumber Co., Hub, N. C.
Nasemond Lumber Co., Moutrose Landing, N. C.
W. B. Blades & Bro., New Berne, N. C.
Stinson Lumber Co., New Berne, N. C.
John L. Roper Lumber Co., Roper, N. C.
S. R. Fowle & Son, Washington, N. C.
Camp Manufacturing Co., Winton, N. C.

W. T. Ferguson, Ferguson's Wharf, Va.
Cooper & Spottswood, Jarratts, Va.
Apex Lumber Co., Lynchburg, Va.
D. S. Jones, Newport News, Va.
Tunis Lumber Co., Norfolk, Va.
George G. Tyler, Norfolk, Va.
Cummer Co., Norfolk, Va.
H. M. Owen, Pungo, Va.
R. G. Dennis Lumber Co., Suffolk, Va.
Mitchell & Steele, Steeleville, Va.
George E. Rurt, Waverly, Va.

WHITE PINE.
Manufacturers.

J. R. Beatty & Co., Quinnimont, W. Va.
Cumberland Lumber Co., Ronceverte, W. Va.
A. E. Huddleston, White Sulphur Springs, W. Va.
Caldwell Land & Lumber Co., Philadelphia, Pa.

CYPRESS.
Manufacturers.

Mohr-Well Lumber Co., Montgomery, Ala.
Cypress Lumber Co., Sherrill, Ark.
Tallapoosa Lumber Co., Sistrunk, Ala.
T. M. McMillan, Stockton, Ala.
J. P. Little, Sumner, Fla.
Cypress Lumber Co., Apalachicola, Fla.
Wilson Cypress Co., Palatka, Fla.
Hillsborough Cypress Co., Harney, Fla.
St. Mark's Lumber Co., Tallahassee, Fla.
Atlanta Cypress Lumber Co., Atlanta, Ga.
Bibb Land & Lumber Co., Cox, Ga.
N. B. Trelise & Co., Patterson, La.
Julius Levin, Alexandria, La.
Burton Lumber Co., Berwick, La.
Louisiana Cypress Lumber Co., Harvey, La.
Lutcher & Moore Cypress Lumber Co., Lutcher, La.
Iberia Lumber Co., New Iberia, La.
McEwen & Murray, Limited, New Orleans, La.
Callahan & Lewis Mfg. Co., Patterson, La.
J. H. Leavenworth, Greenville, Miss.
E. G. Goddard Lumber Co., Logtown, Miss.
R. F. Learned, Natchez, Miss.
Wm. Curphey, Vicksburg, Miss.
W. H. Richardson, Eagle Rock, N. C.
W. D. Hickman, Granite, N. C.
W. B. Ellis, New Berne, N. C.
Hilton & Dodge Lumber Co., St. Simon's Mills, Ga.
Hanson & Smith, Wilmington, N. C.

CYPRESS SHINGLES.
Manufacturers.

John S. Mash & Sons, Luverne, Ala.
Mobile Shingle & Manufacturing Co., Mobile, Ala.
J. D. Cameron & Son, Mobile, Ala.
Stewart & Butt, Mobile, Ala.
Wm. C. Holt, Montgomery, Ala.
The Cypress Lumber Co., Apalachicola, Fla.
Highland Land & Lumber Co., Humphrey, Ark.
Niemeyer & Darragh Shingle Co., Little Rock, Ark.
Carrey & Ollinger, Bagdad, Fla.
Bohemia Shingle Mill Co., Bohemia, Fla.
Wm. A. McCann, Jacksonville, Fla.
Cherokee Lumber Co., Atlanta, Ga.
T. E. Collier, Cordele, Ga.
J. L. Maxwell, Cornelia, Ga.
J. S. Owens, Hawkinsville, Ga.
H. H. Tift, Tifton, Ga.
Julius Levin & Co., Alexandria, La.
B. T. Durham, Leno, La.
Lutcher & Moore Cypress Lumber Co., Lutcher, La.
McEwen & Murray, Limited, New Orleans, La.
Sult & Co., Ahsaki, N. C.
John L. Roper Lumber Co., Norfolk, Va.
L. Miller Shingle Co., Orange, Texas.
Hilton & Dodge Lumber Co., St. Simon's Mills, Ga.
Moore & Swineford, Orange, Texas.

HARDWOOD.
Manufacturers.

Scatcherd Lumber Co., Decatur, Ala.
H. C. Higman & Co., Decatur, Ala.
G. W. Tarwater, Gadsden, Ala.
Sample Lumber Co., Hollins, Ala.
W. O. Knowlton, Paint Rock, Ala.
Lathrop-Hatton Lumber Co., Riverside, Ala.
J. N. Hutchinson, Salem, Ala.
Guthrie Bros., Sulligent, Ala.
Sullivan Timber Co., Wilson, Ala.
Desha Lumber & Planting Co., Arkansas City, Ark.
G. W. Decker, Black Rock, Ark.
Faist & Co., Bryant, Ark.
Southern Land & Lumber Co., Dry Run, Ark.
J. M. Brittan, England, Ark.
Dickson, Cross & Co., Fairmount, Ark.
Geo. H. Munroe, Receiver, St. Francis, Ark.
W. S. Elder, Stonewall, Ark.
S. C. Dowell, Walnut Ridge, Ark.
James Chapin & Son, Cloverport, Ky.
W. L. Pence, Frankfort, Ky.
E. A. Smith, Glendale, Ky.
J. W. Boyd & Co., King's Mountain, Ky.
Maston Lumber Co., Livingston, Ky.
Kentucky Saw Mill Co., Louisville, Ky.
A. H. Rennebaum, Middlesborough, Ky.
J. B. Doney & Co., Amory, Miss.
Ashley & Clement, Hernando, Miss.
B. Crisler, Meridian, Miss.
C. W. Rich, Richburg, Miss.
J. E. P. Boxley, Robinsonville, Miss.
John B. Wheeler & Co., Cooter, Mo.
Hannibal Saw Mill Co., Hannibal, Mo.
Hunter & Dawson, La Grange, Mo.
The Dickson-Mason Lumber Co., Asheville, N. C.

D. C. Way Lumber Co., Haslin, N. C.
J. R. Fowle & Son, Washington, N. C.
Wm. H. Cole, Waynesville, N. C.
Cumberland Manufacturing Co., Harriman, Tenn.
Moore & McFarren, Memphis, Tenn.
Nashville Lumber Co., Nashville, Tenn.
N. L. Johnson, Pennington Gap, Va.
Mitchell & Steele, Steeleville, Va.
Condon-Lane Boom & Lumber Co., Bretz, W. Va.
James Lumber Co., Charleston, W. Va.
H. S. White, Matewan, W. Va.
J. U. Graham, New Richmond, W. Va.
John W. Graham, Central City, W. Va.
Nicola Bros., Nicolette, W. Va.
J. C. Williamson, Williamson, W. Va.
Beaver Creek Lumber Co., Philadelphia, Pa.
Caldwell Land & Lumber Co., Philadelphia, Pa.

Wholesale HARDWOOD Dealers.

W. H. Treworgy, Boston, Mass.
Smith & Blanchard, Boston, Mass.
James A. Wood, Boston, Mass.
Parker & Page, Boston, Mass.
Weston & Bigelow, Boston, Mass.
Palmer, Parker & Co., Boston, Mass.
Litchfield Bros., Boston, Mass.
E. J. Hammond & Co., Boston, Mass.
W. S. Daniel, Boston, Mass.
John M. Woods & Co., Boston, Mass.
Howard Watson, St. Louis, Mo.
Berthold & Jennings, St. Louis, Mo.
Boyden & Wyman Lumber Co., St. Louis, Mo.
Smith & Little, St. Louis, Mo.
Chas. F. Luehrmann Hardwood Lumber Co., St. Louis, Mo.
Bohn-Verdin Lumber Co., St. Louis, Mo.
Druhe Hardwood Lumber Co., St. Louis, Mo.
Smith & Meier Lumber Co., St. Louis, Mo.
Abeles & Tansig, St. Louis, Mo.
Steele & Hibbard, St. Louis, Mo.
James A. Harnett & Co., St. Louis, Mo.
Wyeth Lumber Co., St. Louis, Mo.
Nat S. Rogers & Co., Asheville, N. C.

HARDWOOD.

Wholesale Dealers and Exporters.

Price & Heald, Baltimore, Md.
Price & Co., Baltimore, Md.
Samuel Eccles, Jr., Baltimore, Md.
E. M. Lazarus & Co., Baltimore, Md.
W. W. Welch, Baltimore, Md.
Wm. S. Cross, Baltimore, Md.
J. van Hall, Baltimore, Md.
A. I. Lyon, Baltimore, Md.

STAVES AND HEADING.
Manufacturers.

Stave & Hoop Co., Alexander City, Ala.
Decatur Stave Works, Decatur, Ala.
J. B. Adams, Longview, Ala.
H. C. Stiles & Co., Stiles, Ala.
P. V. De Land, Black Rock, Ark.
Beck & Ellis Bros., Cherry Valley, Ark.
Danforth & McLin, Greenway, Ark.
Springfield Lumber & Cooper's Co., Jonesboro, Ark.
Stetcher Cooperage Works, Jonesboro, Ark.
Jonesboro Stave Co., Jonesboro, Ark.
J. H. Hamlen & Son, Little Rock, Ark.
Little Rock Cooperage Co., Little Rock, Ark.
Wilson Bros., Piggott, Ark.
Tampa Lumber Co., Tampa, Fla.
J. N. Bray, Cecil, Ga.
Kentucky Union Lumber Co., Clay City, Ky.
Dewey Stave Co., New Orleans, La.
George D. Elke, New Orleans, La.

Pioneer Steam Keg Works Co., Brownwood, Mo.
J. H. Bridges & Co., Campbell, Mo.
W. H. Miller Stave Works, De Witt, Mo.
Hall & Frisbee, Dexter, Mo.
F. G. Oxley Stave Co., Poplar Bluff, Mo.
Wm. H. Cole, Waynesville, N. C.
Palmer Mfg. Co., Wilmington, N. C.
Smith & Wood, Dyersburg, Tenn.
Frank Stave & Lumber Co., Frank, Tenn.
Hudson, Ward & Ray, Greenfield, Tenn.
Sherwood Stave Co., Lawrenceburg, Tenn.
McLean & Smith, Martin, Tenn.
Beck & Elias Bros., Memphis, Tenn.
Sharer & Hall, Oneida, Tenn.
J. F. Howard, Union City, Tenn.
Valley Lumber & Stave Co., Richmond, Va.
W. S. Lewis, Carlos, W. Va.
Gauley River Lumber Co., Gauley River, W. Va.
H. Gwinn, Green Sulphur Springs, W. Va.
Waybright & Heiesman, Kendall, W. Va.
Schaffer, Brown & Co., Kingwood, W. Va.
E. O. Felton, Ronceverte, W. Va.

PINS, BRACKETS AND CROSSARMS.
Central Manufacturing Co., Chattanooga, Tenn.

RAILROAD TIES.

W. M. Molton, Saltpa, Ala.
R. T. Coles & Sons, Cushing, Ala.
Sperry & Shortwell, Sedgewick, Ark.
Garrett Lumber Co., Texarkana, Ark.
G. A. & W. N. Lougue, New Orleans, La.
James S. Pope, Baltimore, Md.
A. B. Hubbard, Chatawa, Miss.
J. M. Allen, Springfield, Miss.
J. W. Woodward, Waynesboro, Miss.
J. P. & W. C. Weatherbee, Westville, Miss.
Ellis Paul, Cedar Gap, Mo.
F. M. Woodsmall, Gorin, Mo.
Blackburn, Cox & Co., Fannewell, Mo.
Wellance Lumber Co., Beaumont, Texas.
James G. Harrison, Disputanta, Va.
The Conquest Tie & Lumber Co., Norfolk, Va.
R. V. Dorsey, Hurricane, W. Va.
A. C. Tidd, Murfreesville, W. Va.
J. McKinley & Co., Ravenswood, W. Va.

SPOKES AND HANDLES.

Manufacturers.

Huntsville Spoke & Handle Co., Huntsville
South Calera Mfg. Co., South Calera, Ala.
Frederick Spoke Factory, Frederick, Md.
Hagerstown Spoke & Handle Co., Hagerstown, Md.
G. D. De Jarnette, Mayfield, N. C.
Newton Spoke & Lumber Co., Newton, N. C.
C. J. Dundas, Statesville, N. C.
Yorkville Spoke & Handle Co., Yorkville, S. C.
Johnson Bros. & Taylor, Brownsville, Tenn.
Gallatin Manufacturing Co., Gallatin, Tenn.
Wells & Lesh, Jackson, Tenn.
Southern Spoke & Rim Co., Memphis, Tenn.
Lewis & Babcock Mfg. Co., Nashville, Tenn.
American Handle Co., Ltd., Knoxville, Tenn.
Hardy & Holmes, Christie, Va.
R. T. Slusser, Fincastle, Va.
N. Poulson, Round Hill, Va.

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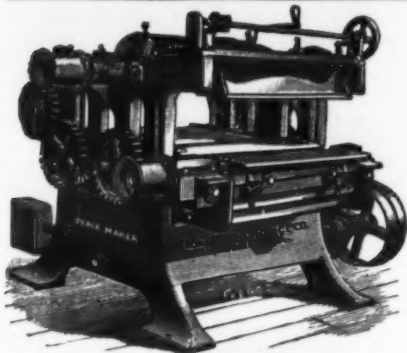
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EASTERN BRANCH, Cor. 6th and Lewis Sts., E. R., New York



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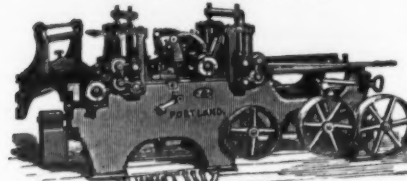
WILLIAMSPORT, PENNA., U. S. A.

Manufacturers of the Finest and Most Complete Line of

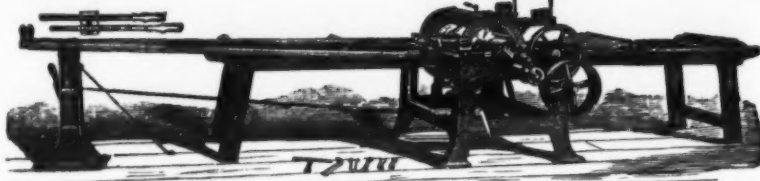
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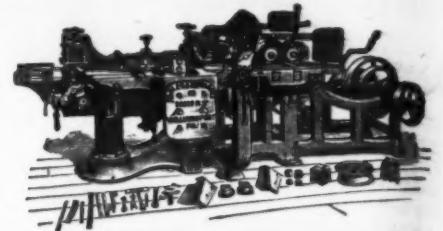
In AMERICA.



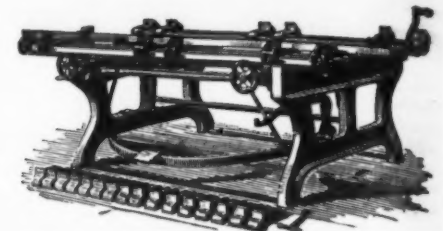
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No. 2 New King Power Feed Gang Rip Saw.



Iron Column 12 in., 10 in. and 9 in. Four Sided Moulder.



Improved Door Clamp.

"NO PLACE LIKE HOLMES" FOR WOOD WORKING MACHINERY

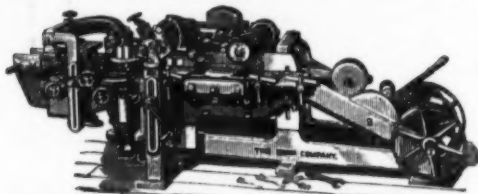
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Where'er we may roam,
For Re-Saws and Edgers,
There's none like E. B. Holmes.*

E. & B. HOLMES MACHINERY CO.

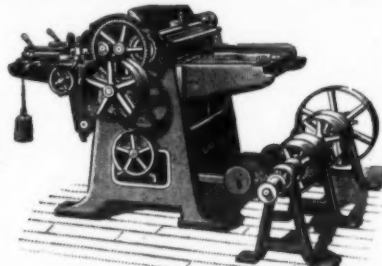
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BUFFALO, N.Y.

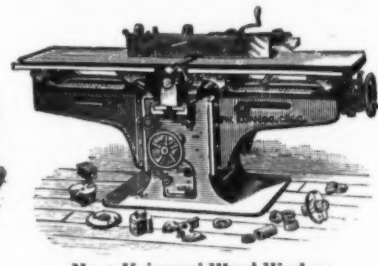
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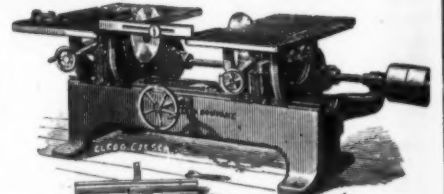
Heavy 12-inch Standard Four Sided Molder.



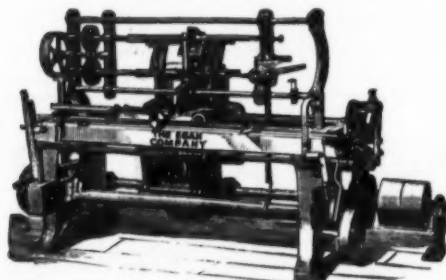
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No. 1 Universal Wood Worker.



No. 2 Double Cutting Off Saw.

Improved Automatic Spoke Lathe.
Capacity in excess of any other spoke lathe

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228-248 W. Front Street,

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No. 3 Variety Saw,
with Tilting Table and Boring Attachment.

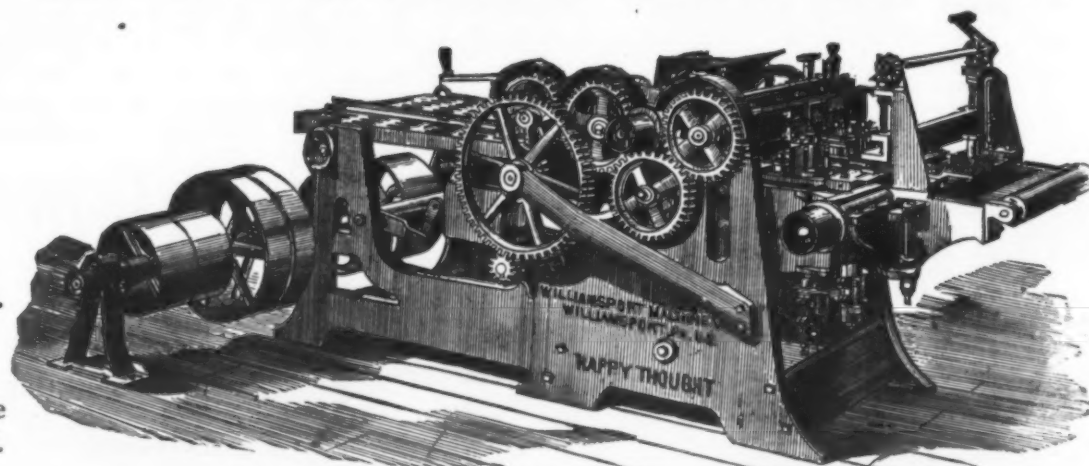
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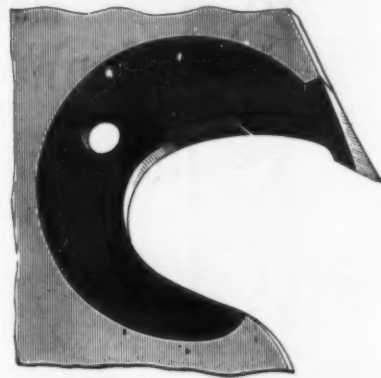
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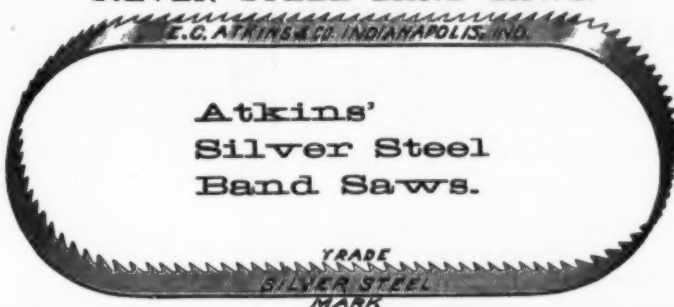
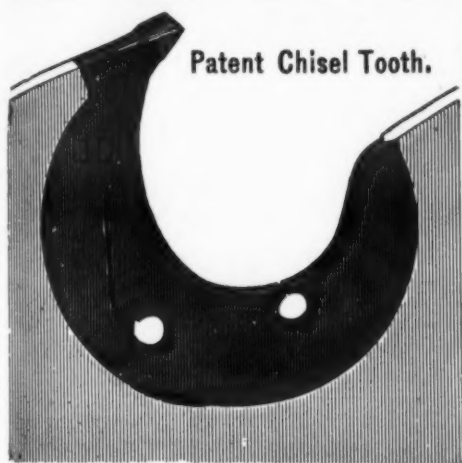
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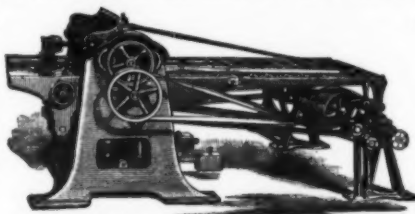
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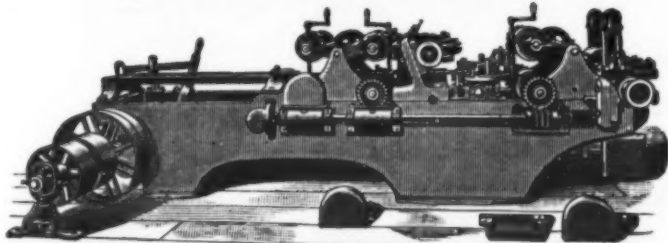
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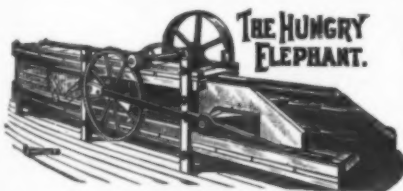
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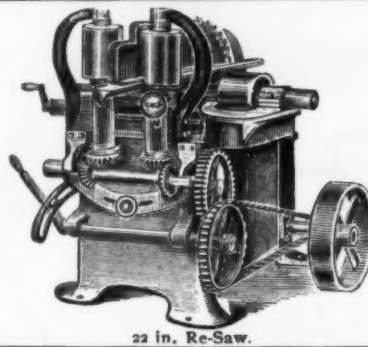




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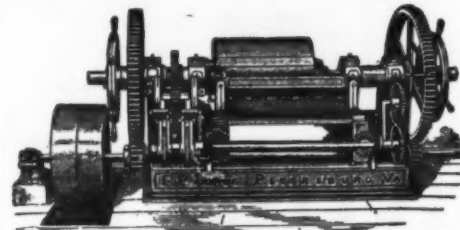


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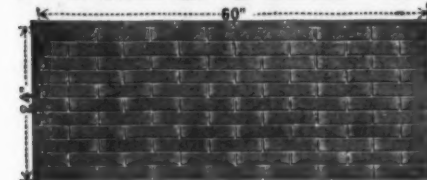


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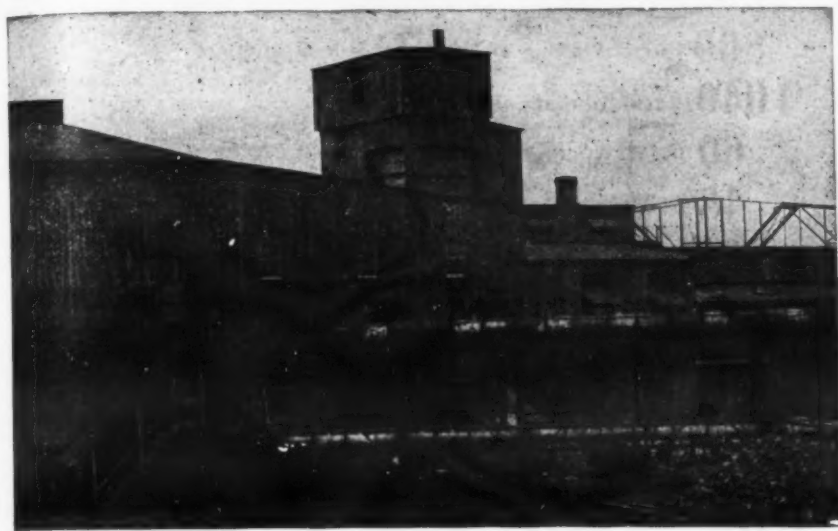
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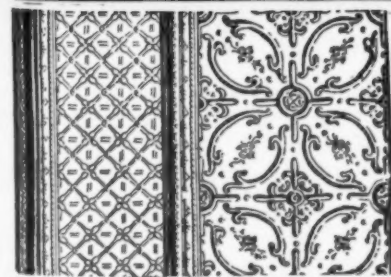
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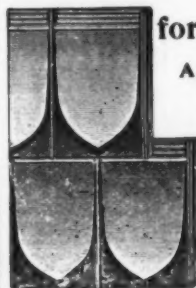
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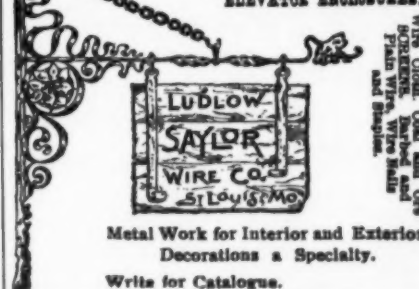
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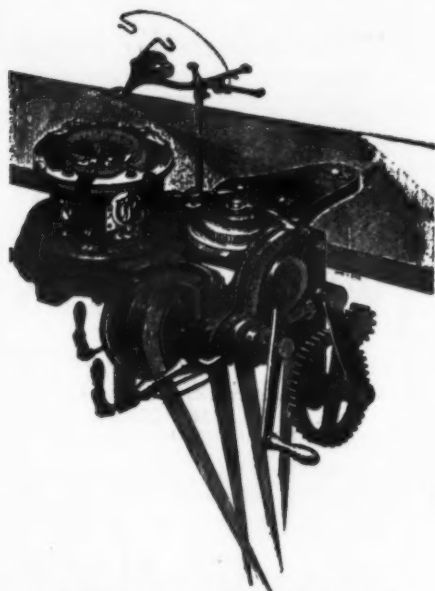
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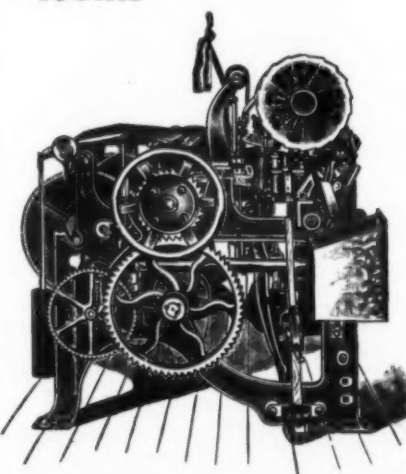
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A mill that orders common looms at the
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We now recommend this loom and stake
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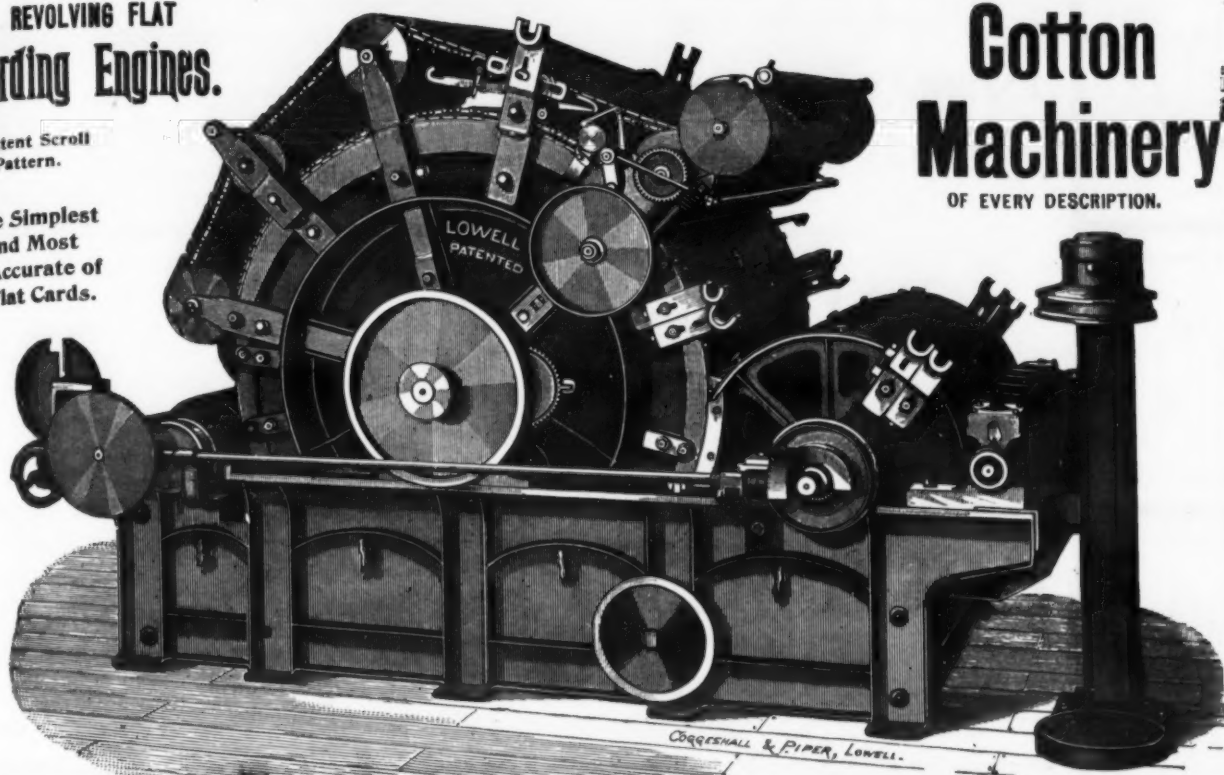
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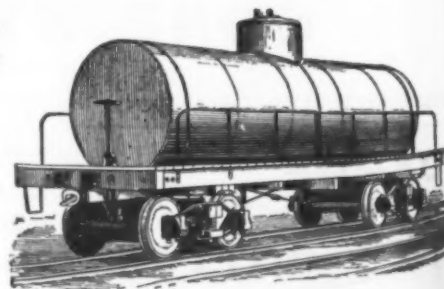
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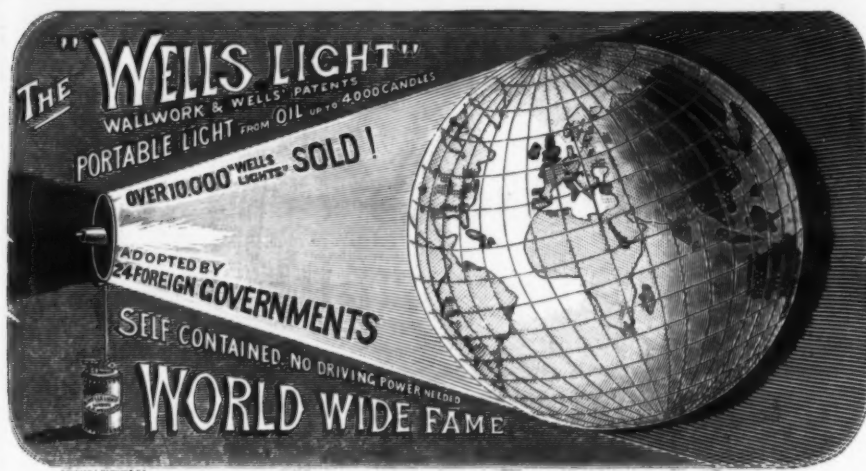
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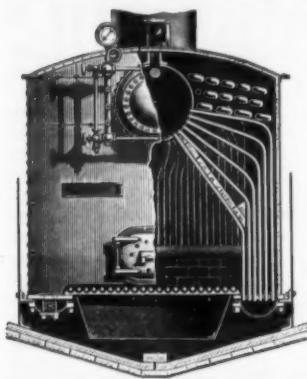
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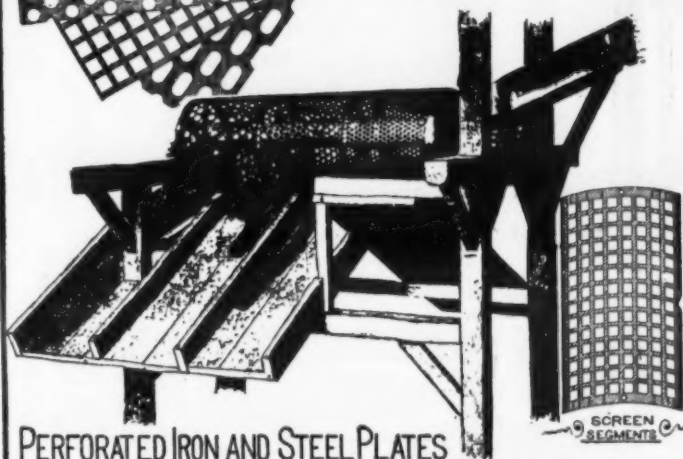
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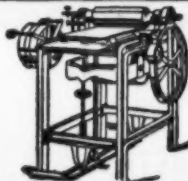
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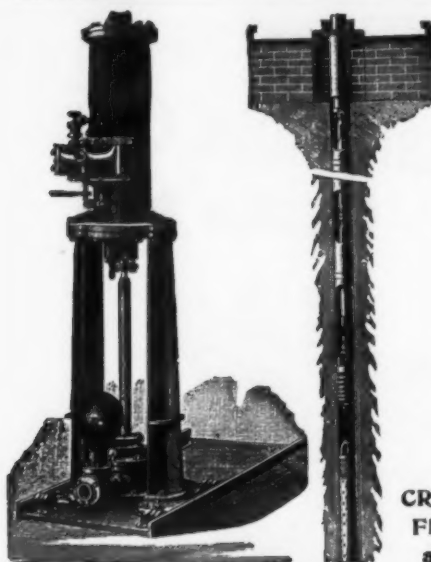
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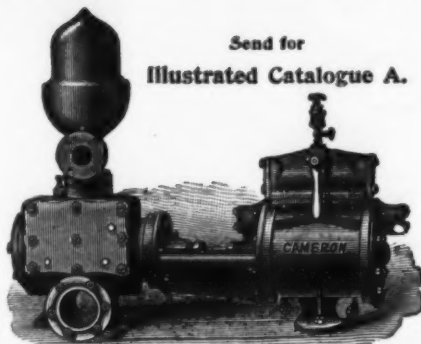


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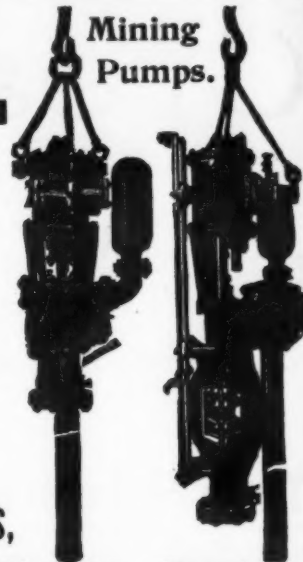
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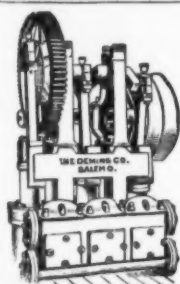
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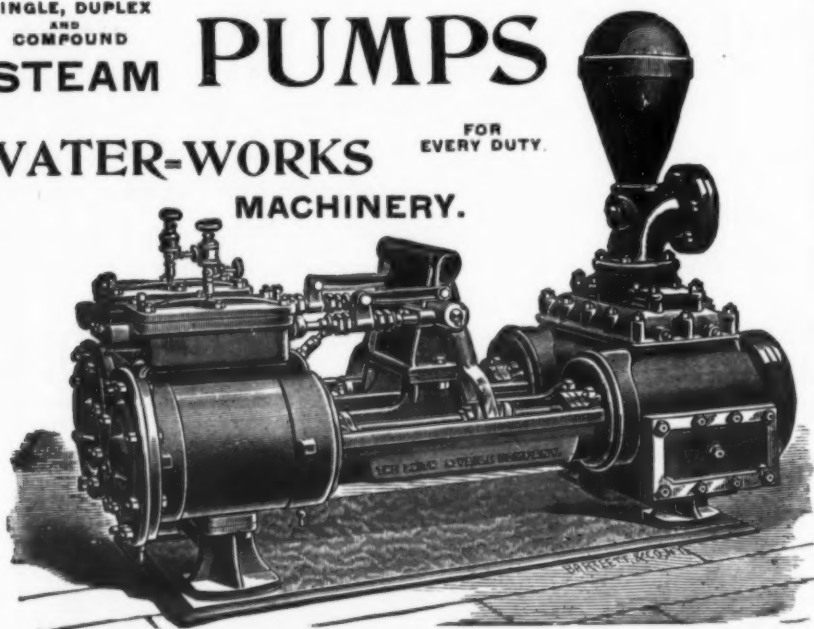
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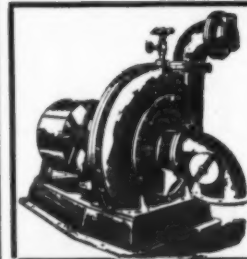
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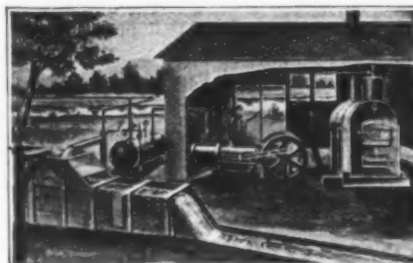


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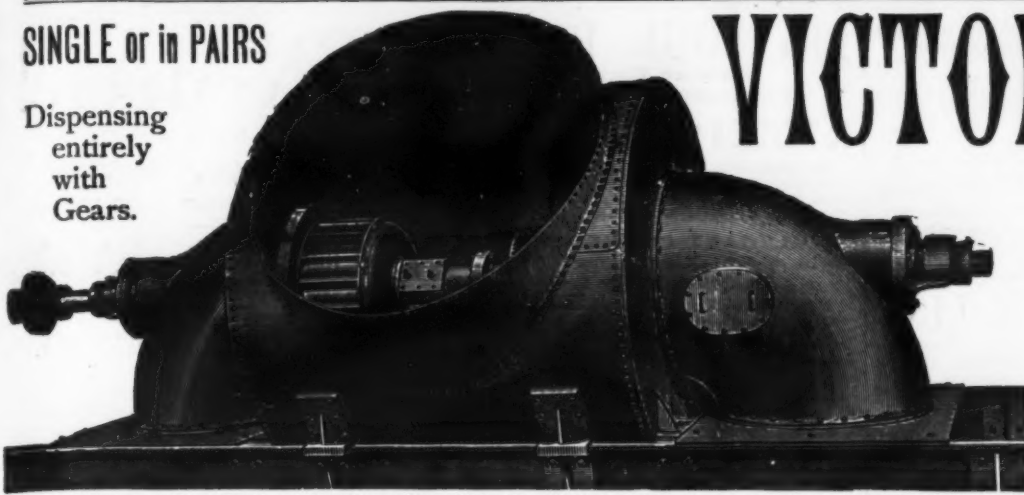
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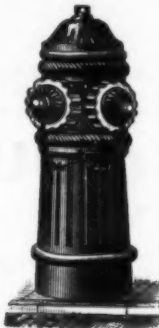
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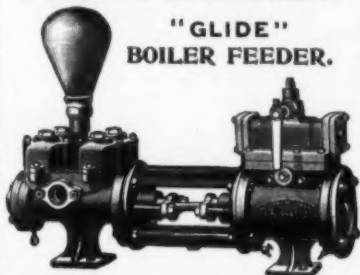
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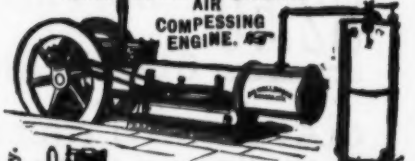
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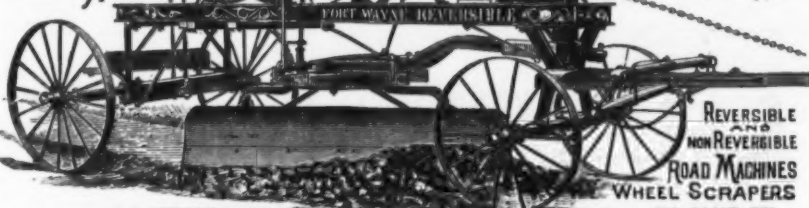


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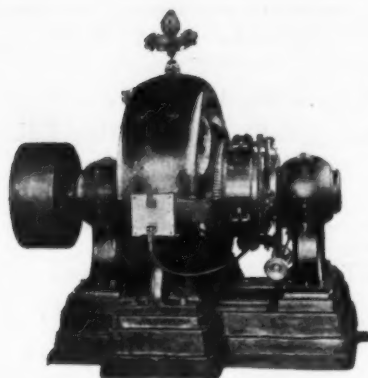
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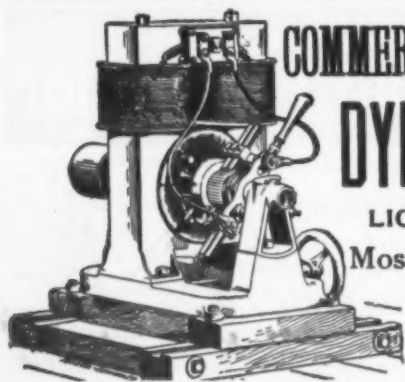
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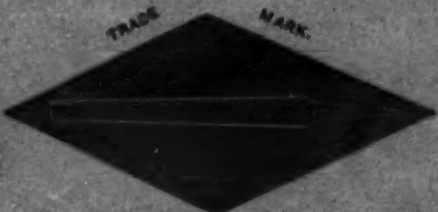
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